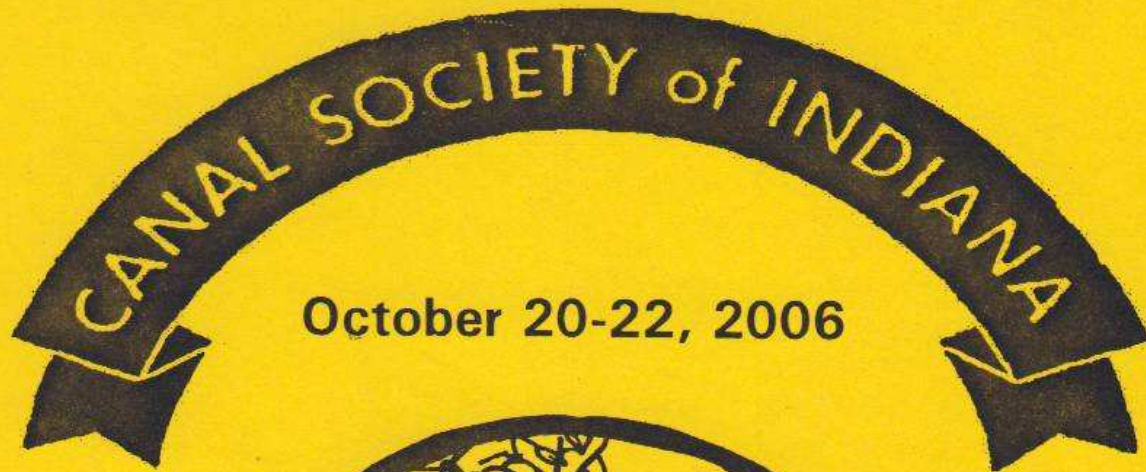


WHITEWATER CANAL

CINCINNATI & WHITEWATER CANAL
HAGERSTOWN EXTENSION



Harrison, Ohio

"CANAL PASSAGES"

CANAL SOCIETY OF INDIANA PO BOX 40087 FT WAYNE, IN 46804

FOREWORD

CANAL PASSAGES

“Canal Passages” was thought to be an appropriate theme since both the Whitewater Canal and the Cincinnati & Whitewater Canal pass

Each tour given by the Canal through the Whitewater Valley; ca- Society of Indiana attempts to relate nal boats passed through the Cleves the history of the canal and point out Tunnel, the Duck Creek Aqueduct, the location of its structures in a par- and the canal locks; General John ticular area of Indiana or in a Hunt Morgan passed through Indi- neighboring state. The tours also in- ana burning the bridge across the ca- clude other canal related buildings nal slackwater at West Harrison; and and the early history of the counties a new trail passes along the canal. and towns being toured. They usu- ally have a theme - something that ties the area together.

Previous tours of the Whitewa- stored train of the Whitewater Valley ter and the Cincinnati & Whitewater Railroad, think back to the more re- Canals have focused on the canal re- laxed time of 1830s - 1870s when mains from Hagerstown to Law- horses pulled boats. Take time to renceburg, Indiana, and from Harri- stroll along the quiet towpath trail, son to Cleves, Ohio. It was decided observe the butterflies and wildflow- that this tour would investigate the ers, and listen to the birds and water canals from Metamora to the Ohio as it ripples through the canal re- River with emphasis on the junction mains. Make this your passage of the two canals at West Harrison, through the valley. Indiana / Harrison, Ohio and the building of a Whitewater Canal Trail from Metamora to Brookville.

Although today we usually pass through the valley by car or motor coach or perhaps ride the re-

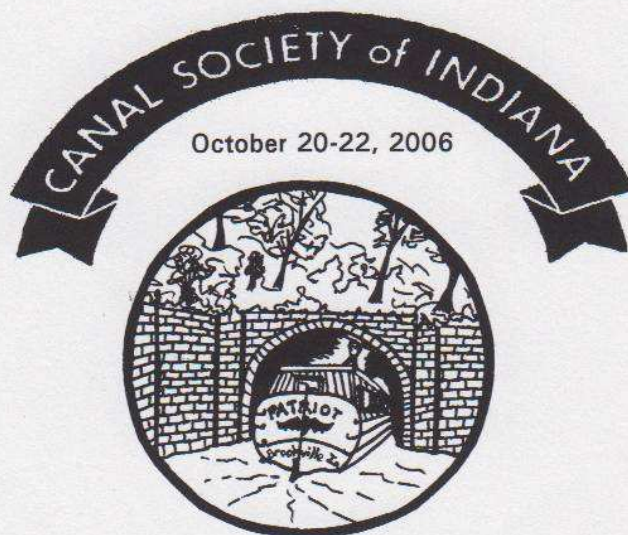
stored train of the Whitewater Valley Railroad, think back to the more relaxed time of 1830s - 1870s when horses pulled boats. Take time to stroll along the quiet towpath trail, observe the butterflies and wildflowers, and listen to the birds and water canals from Metamora to the Ohio as it ripples through the canal re- River with emphasis on the junction mains. Make this your passage of the two canals at West Harrison, through the valley.

Welcome aboard!

Carolyn I. Schmidt



**WHITEWATER CANAL
CINCINNATI & WHITEWATER CANAL
HAGERSTOWN EXTENSION**



Harrison, Ohio
"CANAL PASSAGES"

EDITOR:

CAROLYN I. SCHMIDT

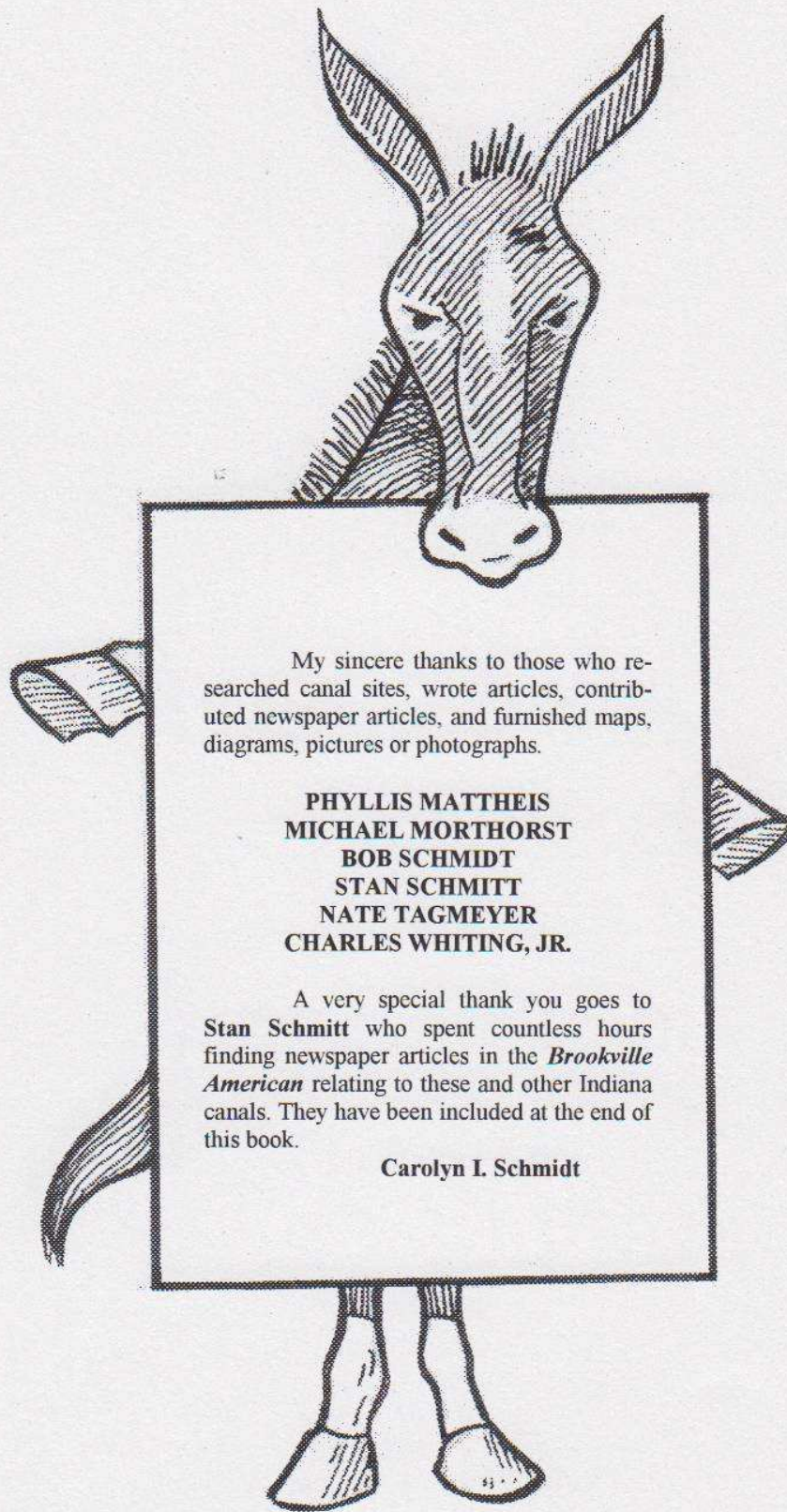
RESEARCHERS:

PHYLLIS MATTHEIS
MICHAEL MORTHORST
BOB SCHMIDT
CAROLYN SCHMIDT
STAN SCHMITT
CHARLES WHITING, JR.

LOGO DESIGN:

CAROLYN SCHMIDT

Copyright 2006
CANAL SOCIETY OF INDIANA P.O. BOX 40087 FORT WAYNE, IN 46804
E-mail INDCANAL@AOL.COM Website INDCANAL.ORG



My sincere thanks to those who researched canal sites, wrote articles, contributed newspaper articles, and furnished maps, diagrams, pictures or photographs.

**PHYLLIS MATTHEIS
MICHAEL MORTHORST
BOB SCHMIDT
STAN SCHMITT
NATE TAGMEYER
CHARLES WHITING, JR.**

A very special thank you goes to **Stan Schmitt** who spent countless hours finding newspaper articles in the *Brookville American* relating to these and other Indiana canals. They have been included at the end of this book.

Carolyn I. Schmidt

TABLE OF CONTENTS

Page

6.	Structures of the Hagerstown, Whitewater, and Cincinnati & Whitewater Canals with Their Locations
14.	History of the Whitewater Canal
16.	Hagerstown Extension Canal Contractors
18.	History of Hagerstown
20.	James Heron Letter to Gov. James Whitcomb
23.	History of Cambridge City
23.	The Vinton House
29.	History of Milton
32.	Accident at Internal Improvement Celebration - Newspaper Article by S. W. Parker
33.	History of Connersville
40.	Canal in Winter at Connersville
41.	History of Nulltown
41.	History of Berlin
42.	Ice Ponds
43.	History of Alpine
43.	History of Laurel
46.	Whitewater Canal Boat Names
47.	M. S. Webb's Daybook - Dam and Bridge Specifications
48.	Newspaper Articles from the Brookville American
49.	History of Metamora
51.	Metamora's Historic Buildings Plaques
52.	History of Brookville
54.	Brookville Internal Improvement Celebration
57.	Reminiscences of Josiah McCafferty
58.	Whitewater Canal Newspaper Articles
59.	History of Cedar Grove
61.	Excerpts from Erasmus Gest Letters While Working on Whitewater Canal
63.	History of New Trenton
63.	History of West Harrison
63.	History of Lawrenceburg
64.	First Flouring Mills in Lawrenceburg
68.	Table of Canal Tolls
69.	Whitewater Canal Newspaper Articles
72.	History of the Cincinnati and Whitewater Canal
73.	William Henry Harrison Letter to M. T. Williams
77.	Cross Section of Cleves Tunnel
78.	Diagrams of Canal Structures
81.	Bibliography
<u>Addenda</u>	
83.	The Dam at Harrison for the Whitewater Canal
89.	Lawrenceburg River Walk
90.	Jesse Hunt House
91.	Old Elm Row, The Tumbles
93.	Whitewater Canal Trail
94.	Newspaper Articles from the Brookville American 1836-1850
113.	Letter from Connersville
114.	Newspaper Articles from the
126.	Letters, Pictures, Newspaper Articles, etc. mentioning the Whitewater Canal

The first 81 pages of this book are from the CSI "Towpath Tracks" tour in September 2001.
New material found since that time is in the Addenda.

The first 81 pages of this tour guide are from the September 2001 tour guide, “Towpath Tracks,” that can be found on this website. Additional new information is found here in the following addenda.

THE DAM AT HARRISON FOR THE WHITEWATER CANAL

Charles Whiting, CSI director from Greendale/Lawrenceburg, has searched newspapers and documents to determine if they ever rebuilt Dam #1 at Harrison after the second flood of 1847. That is when the Cincinnati and Whitewater Canal was raised to be fed from Dam #2 since Harrison to Lawrenceburg was out of service. He found that the Lawrenceburg newspapers were missing after March 24, 1849 but located ones for 1850. The papers indicate it was rebuilt. He also found a description of the Harrison Dam in the notebook of Erasmus Gest in the Cincinnati Historical Library collection and a file on Dam 1. These follow:

March 24, 1849 *Indiana Register* Lawrenceburgh
White Water Valley Canal.

Editor of Register:

Sir —Much has been said and but little done in the months past by our citizens in relation to making repairs on the Canal from this place to Harrison. All feel and acknowledge the importance of the work, but heretofore no plan has been devised by which so desirable an end could be obtained. At length, however, a plan has been chalked out which, with the united efforts of our citizens acting in concert with the White Water Valley Canal Company, the work can be completed by the first day of July next. The Company have appropriated \$40,000 to the repair of this portion of the canal and propose to commence the work immediately, provided the City of Lawrenceburgh, will loan them \$6,000 to enable them to complete the work, for the payment of which they agree to pledge all the tolls and water rents on this end of the Canal until the \$6,000 and interest is paid up. They will also issue the bonds of the Company, to be held by the City, until the debt is paid. Mr. Shoup, agent for the Company, proposes in his individual capacity to guarantee the payment of the amount by mortgage on his property, providing the City would pay a bonus of 1000. By taking the later proposition the City could under no circumstances lose anything but the bonus.

The plan for raising the money to be loaned to the Company is this. The City to issue her bonds in amounts to suit purchasers, say from \$25 to \$100. These bonds to bear an interest of 6 per cent, redeemable in five years —an order to be passed on the books of the City Council, however, appropriating all the tolls and water rents as fast as received to the payment of the \$6,000 of bonds, pro rata.

When the Canal was in operation the receipts at the office in this place amounted to \$4,000 per annum. The receipts are not likely to be less than that amount, when it shall again be in repair; besides we should also

have the tolls from Harrison to this point which may very safely be put down at \$2,000. This will pay the principal of the \$6,000 loan in one year.

These bonds when issued are to be cashed by our citizens and held by them, until paid as above stated. But before any issue of Bonds can be made a vote of the citizens must be taken —an election for that purpose having already been ordered to take place on Monday next, at which the citizens holding two-thirds of the property must vote "for the issue of bonds" or no bonds can be issued. Property held by non-residents not represented, will be counted against the measure. Now we do not believe any citizen will vote issue of bonds directly, but remember to stay away from the polls your property represented, and consequently counted against the issue of bonds. Then we say, see to it. Put your shoulder to the wheel as all good citizens should. Let the Canal be repaired; the Cincinnati and St. Louis and the Rushville Rail Roads be built and Lawrenceburgh, with her Ohio river, Rail road, and Water power, will be behind no town in the State. Let no one fold up his arms and say nothing can be done. We say the Canal can be repaired and will be done at once, if we all do our duty. Then up and at it.

A CITIZEN Lawrenceburgh, March 23

Daily Enquirer Cincinnati April 1, 1849

Lamentations of the Lawrenceburghers

The Lawrenceburgh, Ia., *Register* thus records the calamities of that beautiful place:

"We are poor now, and have been unfortunate. We have been downcast. A combination of circumstances, both fire and flood —have disabled us, and we have for two years been brooding over our misfortunes. We lost our canal by one flood, — we lost our distilleries by another flood, —we lost one of our best flouring mills and our oil mill by fire —we lost many of our citizens and a good deal of property by the flood of '47."

"But misfortune has not altogether destroyed hope or banished enterprise, and we are pleased to say, that the prospects of a recovery from all the calamities that have been afflicted on it, by both fire and flood, is very cheering."

November 15, 1850 *Independent Press* Lawrenceburgh

Imports: Lewis & Eichelberger 2400 bushels of
Wheat by canal

Shipment: 500 barrels of flour to Harrison by canal
boat York

November 22, 1850 *Independent Press* Lawrenceburgh

The Whitewater Canal is in good order and boats continue to arrive and depart but we may from present indications look for a freeze up.

Imports by Lewis & Eichelberger 1650 bushel of
wheat by canal

THE WHITEWATER CANAL—ADDENDA

Erasmus Gest Notebook

White Water Canal

The part between Brookville & Lawrenceburg was put under contract during the fall of 1836. Col. Simpson Torbet, Resident Engineer, Elisha Long Acting commissioner. Martin Coryell Leut Assistant. T. G. R. Noel & Clement Dare Assistants. Length of Line 30 Miles & 36 ch. Lockage from surface of Water in to Pool of Dam across East fork River to surface of Water in Bain at Lawrenceburg 141 1/2 feet = 4.60 feet per mile (36 chains = 4.5 miles, 141.5 / 30.45 miles = 4.6 feet per mile) From thence to low water in the Ohio River 48 feet. High water of the Ohio in 1832 63 feet above low water. 17 Lift Locks & 3 Guard Locks

Estimated Cost

Cost at Contract Prices are offered
Estimates in Nov. 1838

Cost When Complete \$603,759.95 1/2

Money Expended on the White Water Canal
Construction Expenditures

from Commencement up to Dec. 4th 1838	\$564,728.68
From commencement up to M 1st 1837	\$ 1,186.—
Ap 1st to Nov 24th 1837	205,857.06
Dec 8th 1837 to Dec 4th 1838	<u>357,685.62</u>
	\$564,728.68

Contingent Expenses from the Commencement to Nov 30th 1836	\$ 4,645.
Nov 30th to Dec 1st 1836	2,614.
Dec 1st to Nov 30th 1837	8,210.
Dec 1st to Dec 1st 1838	<u>11,074.</u>

Total	\$ 26,545.
Cost per mile about	\$ 20,000.

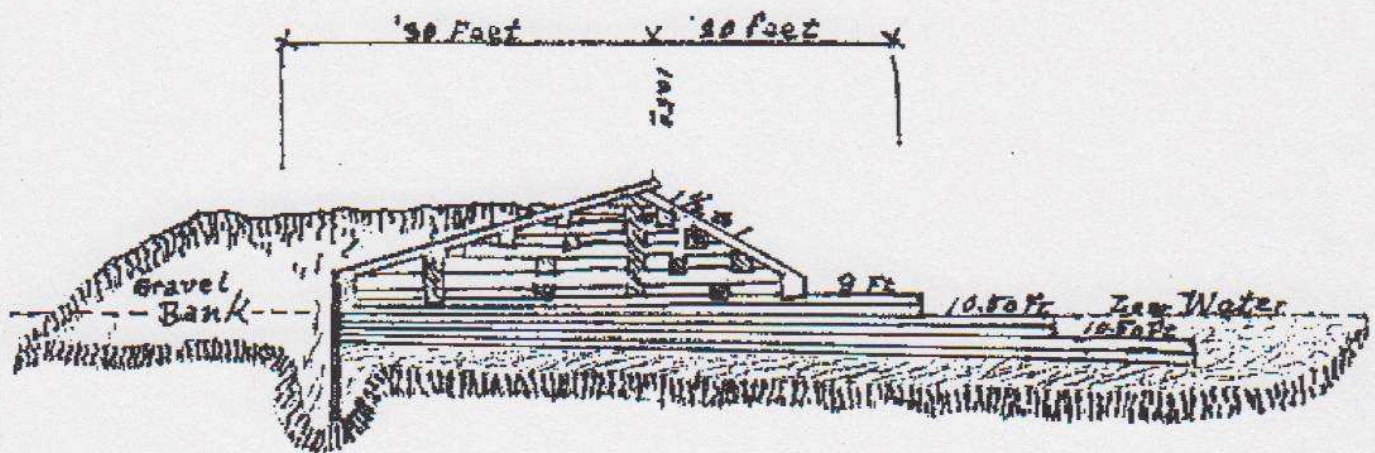
DAM

Dam pit excavated 5 Feet below Low Water. Comb of Dam 8 Foot Above Low Water. Covering Timber 6 inches thick. Range timber 12 inches square. Cross ties 12 inches in Diameter. Round Poles will average about 7 inches Diameter on the top course must be White Oak. The brush must be put in the Pit to the thickness of 3 Feet when compressed with the weight of a Gentleman. Cross Ties will be placed 8 Feet from centre to centre. The range timber must be well joined together by skipplaping. The covering timber must be Fastened to the Range timbers with wrought spikes 14 inches long and 1/2 inch square. Wings of the Abutments extend out 16 Feet from the (?)

Plan of Dam as built across the White Water River
Near Harrison, Ohio by the State of Indiana
Length of Dam 376 feet
Height above low water 8 ft.

The width of the river at the entrance of the canal above this dam some 400 yards is 397 feet there is a bridge built across at that point for wagons & for Horses it is on the Long's Jackson bridge

Plan of 3 aprons of 130 feet each



Dam 1 File — (?)

Bill of materials for Dam No. 1

104 Lineal feet will be covered with Timbers 12 ft long and 2 inches deep and not less than 9 inches high to be dressed on all sides = 1248 cubic

48 Lineal ft will be covered with timber 9 ft long 12 in deep and not less than 9 in wide to be dressed on all sides = 432 | 1680

1380 Square feet 2 in plank for foundation

1000" " 2 " " for sheet piling

Bill for Base

470 Round Trees from 8 to 10 inches at the but from 55 to 60 ft in length

470 Round Trees from 8 to 10 inches at the but and from 50 to 55 ft in length

470 Round Trees from 8 to 10 inches at the but and fro 45 to 50 ft in length

470 Round White Oak trees from 8 to 10 inches at the but and from 45 to 50 feet in length

Brush sufficient to cover the base of the dam, from 1 to 2 inches in thickness

Superstructure

10900 Lineal feet. range timber from 10 to 12 inches square

White Oak and in length not less than 18 ft and as much longer as practicable.

To splice on the ties

#48 Round ties not less than 15 inches at the but 45 ft long (Expunged)

48 Round ties not less than 15 inches at the but 37 1/2 ft long

48 Round ties not less than 15 inches at the but 30 ft long

48 Round ties not less than 15 inches at the but 22 1/2 ft long

48 Round ties not less than 15 inches at the but 15 1/2 ft long

1500 Square feet of plank for covering the upper side of Dam 6 in thick in lengths of 20 ft

48 Square feet of plank for covering the upper side of Dam 6 in thick and in lengths of 13 (?) ft

THE WHITEWATER CANAL—ADDENDA

Length of Dam between abutment 375 feet

1940 (?) Square feet of Timber for covering the lower side of the Dam 6 inches thick & in length of 18 1/2 ft

1100 (?) White Oak or Locust Pins 18 in long & 2 in in diameter

1875 Wrought Spikes 14 in long & 5/8 square

1950 Square feet 2 in Plank for Sheet piling at upper end of dam

The covering timber must be of good White Oak

The plank may be of Sycamore or White Oak

1500 Pins for top covering 15 inches long & 1 3/4 in diameter

The Changed Plan

9025 Lineal feet of Range Timber

5250 Square feet 6 in White Oak plank 14 ft long for covering upper slope

4875 Sq feet in White Oak plank 13 ft long for covering upper slope

5815 Sq ft in White Oak plank 15 1/2 ft long for covering lower slope

2| 15940

7970 cubic ft

This item to be left out of the bill

Garce (?) Ridde Honish (?) & Co. 1800 spikes _____ unable to read _____

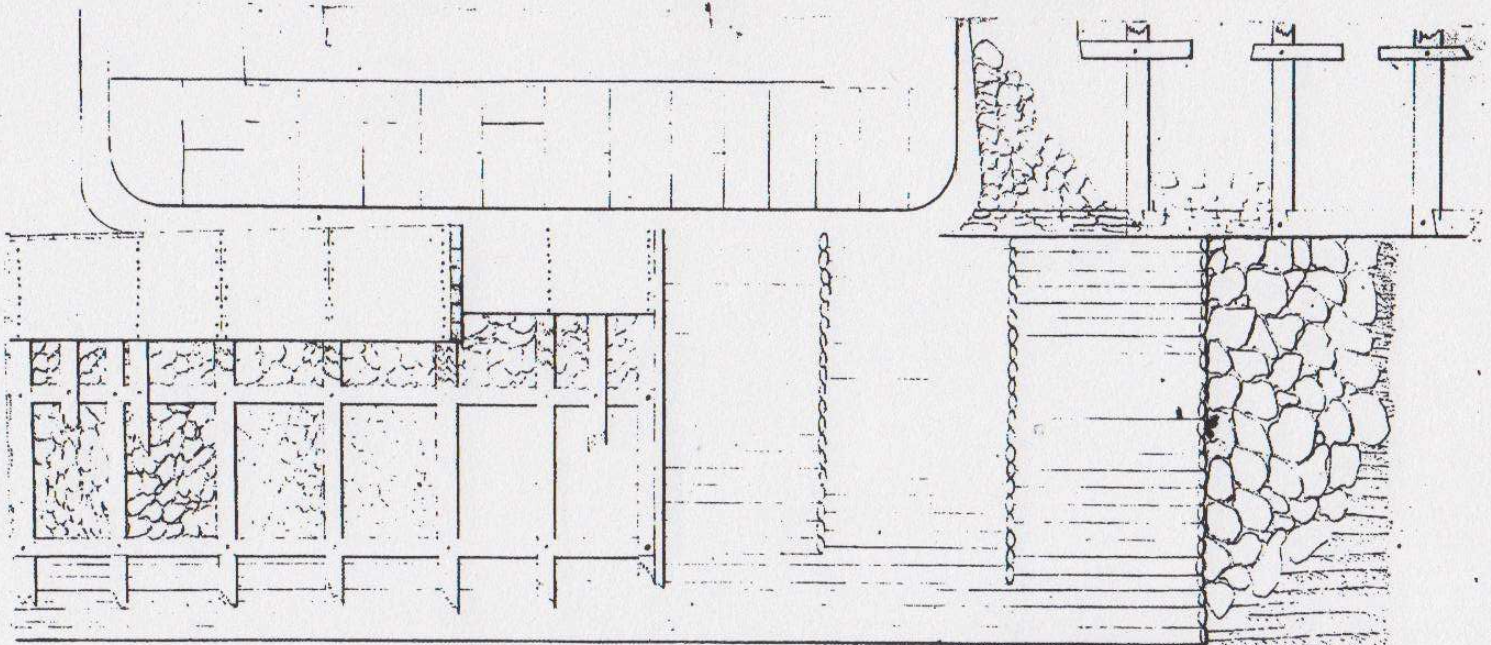
The Plan of Dam No 2-1 White Water Canal on the next page has this information given on the right side of the diagram:

Reference

From comb of Dam to foot of Lower slope 12 feet
From comb of Dam to foot of Upper slope 24 feet
From foot of lower slope to end of top trees 9 feet
From this to end of next step 10 1/2 feet
From this to end of next step 10 1/2 feet
Brush will extend below end of trees 10 feet
Spikes per lower slope must be rugged
4 large iron bolts will be driven through the rabbit stick into each of the 72 inch poles 2 feet long.
The 12 inch poles will be spaced 8 feet apart & the Rabbit Stick bore 3 inches into them.
The lower covering must be fitted very evenly at top and bottom and must bear very firmly on every linear timber

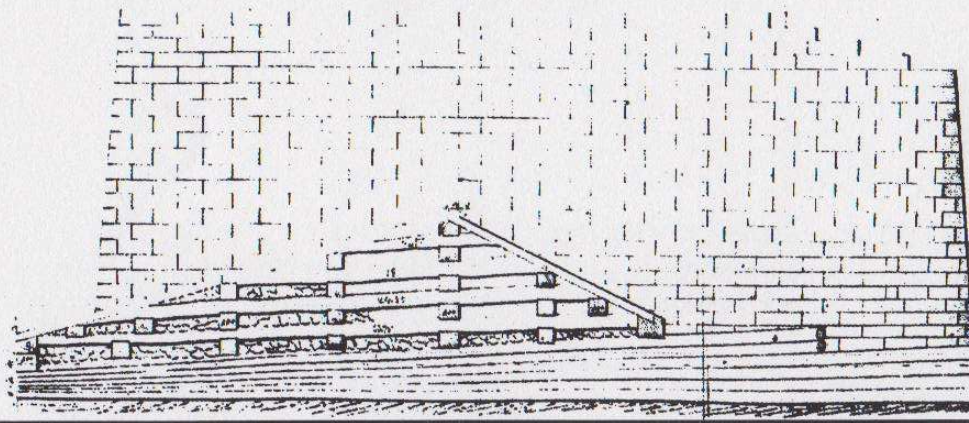
Top of the Abutment 15 feet a Bottom 7.30 B

Low Water 4.50 B



PLAN OF DAM N^o 2-1

White-Hozer Canal.



Reference

From crest of Dam to foot of lower slope 10 feet
 Upper 25
 From foot of lower slope to end of top level 5
 From this to end of upper slope 100
 100

Should well extend below end of base 10
 Spikes for lower slope must be staggered
 A large lean hole will be of place having the
 side which falls east of the 10 feet hole a good line
 The 10 feet hole is to be 5 feet apart of the Rabbit Hole
 and 2 inches into them.
 The lower covering must be filled every second at
 top and bottom, and must have every second an every
 lower timber

Top of the structure is to be 10 feet high.

See page 400

SPECIFICATION

FOR BUILDING DAMS ON THE

WHITEWATER CANAL.

DAM NO. 1.

The Dam will be raised 11 feet high from low water, and will be 375 feet in length between abutments. Its base, measuring with the stream and including apron, will be 78 feet, that is, 48 feet below the comb or spill of the Dam and 30 feet above that point.

The whole base of the structure as above described, will be excavated to the depth of 3 feet below low water. The Dam will then be commenced by covering the whole extent of its base with fine brush to be laid on in such quantities as to make a thickness of about 2 feet with the weight of a man standing on it—the brushing to be commenced at the lower side and to be placed in regular tiers of the full thickness, with the tops always on the bed of the stream, and the butts of each tier on the tops of the tier next below it.

Unhewn trees from 50 to 60 feet long, 8 to 12 inches diameter at the stump, and as straight as the forest will furnish, will next be brought on and laid with the current, the tops being placed up stream, and the butts or stump ends projecting below the crib for the purpose of an apron. With these the Dam will be raised to the height of one foot above low water; the several layers of trees to be so placed that the butts will form a succession of benches or steps to receive and break the descending floods. The bottom courses of this tree foundation or apron will extend 33 feet and the upper courses 8 feet below the foot of lower slope. The upper courses of trees, amounting to about one third the whole number, must be oak; the remainder may be any kind of sound hard timber. The intesticies in the trees and brush will be filled with small stone and coarse gravel as the work progresses.

Upon this foundation of trees and brush, a crib will be erected about 10 feet high, with 45 feet base, having a slope of 1½ base to one rise on the lower side, and 3 feet base to one rise on the upper side, drawn to a point at top like the roof of a cabin. The range timbers of the crib stretching across the stream will be

hewn on three sides and will be placed about 7½ feet apart from centre to centre. The cross ties will be unhewn, except where they intersect the range timbers, and will be placed 8 feet apart.

The range and cross timbers where they cross each other will be strongly locked together by notching dovetailing and treenailing. The crib will be filled with stone. Both the upper and lower slopes of the crib will be covered with six inch plank, jointed with a water joint and secured with spikes 14 inches long and 5-8 squares. At the upper side of the crib a row of sheet piling will be driven 2ft below base of dam, connecting at top with foot of covering timber so as entirely to cut off the water either through or under the structure. The dam will then be finished by placing a large bank or mound of course gravel above the crib, at least 8ft deep on the foot of the upper slope.

Should it be found necessary the bed of the stream at the most suitable point below the Dam will be raised one foot with stone & brush so as to place the whole of the timber foundation under water, and thereby prevent the possibility of decay.

The abutments will be 50ft long, with wings both at the upper and lower end extending 24ft into the bank at right angles to the face. They will be formed of coursed hammer dressed masonry. (See specification for masonry.)

DAM No. 2.

This dam will be 14ft high from low water and 285ft long between abutments. The plan and manner of its construction will be the same generally with that just described, for Dam No. 1, (see plan.)

DAM No. 3, across the East Fork,

Will be 11ft high and 190ft long between abutments. The base of the Dam, the form of crib and the mode of construction, will correspond in every particular with the specification for DAM No. 1.

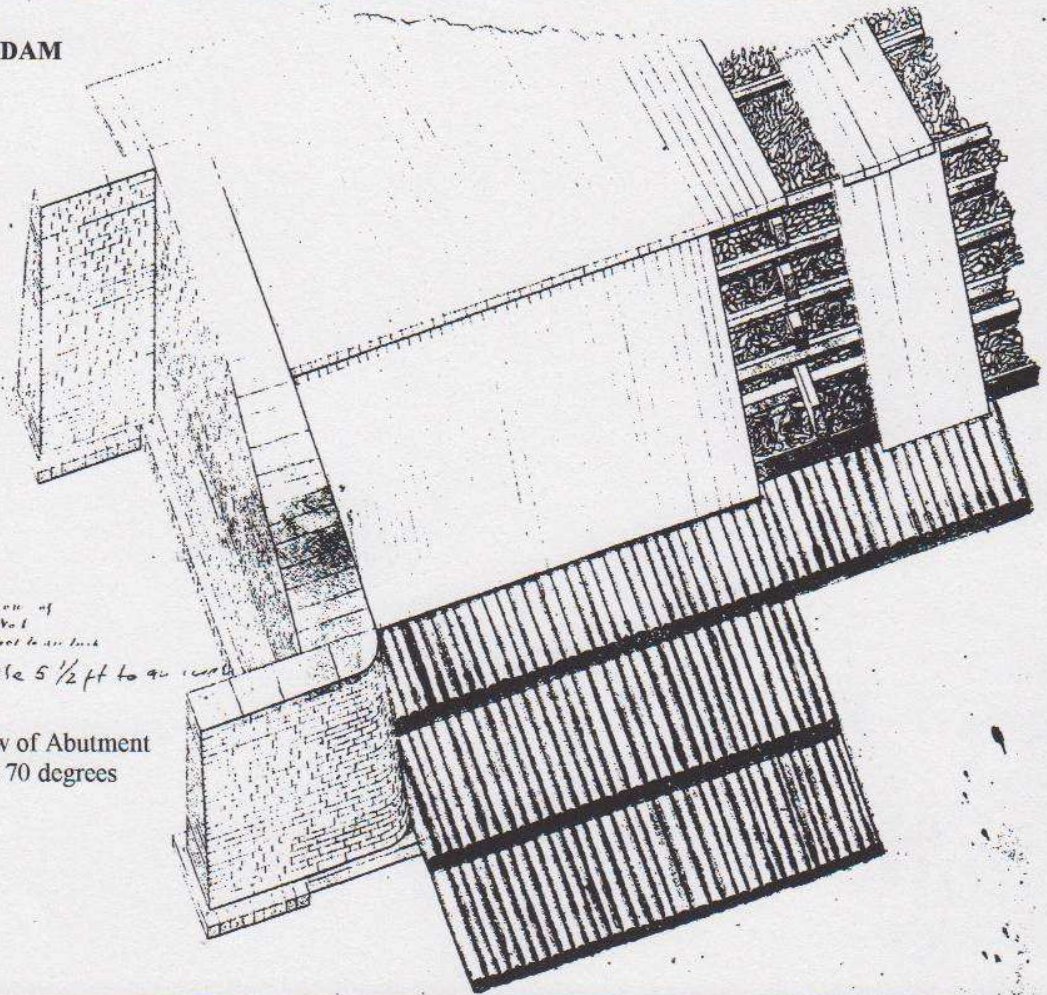
The bed of the stream where these dams are to be constructed is suppose to be gravel and sand.

THE WHITEWATER CANAL—ADDENDA

WHITEWATER CANAL DAM

*Isometrical Perspective View of
Abutment and Part of Dam - Vol
Arch. of the River - Scale 5 1/2 ft to an inch
Angle 70 degrees*

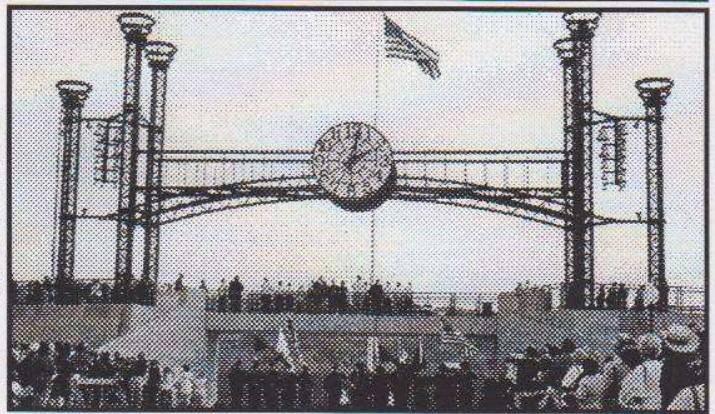
Is a metrical Perspective View of Abutment
and Part of Dam No. 1 Angle 70 degrees
Scale 5 1/2 ft to an inch



LAWRENCEBURG RIVER WALK

On September 8, 2002, ground was broken and plans unveiled for the "War Memorial to the Common Man" to be built on the Ohio River levee at the end of Walnut Street. [The memorial is located two blocks downstream, Southwest, from the Whitewater Canal basin and at the end of the block from the Jesse Hunt House in Lawrenceburg.]

The \$2.5 million memorial features 7-foot-tall bronze statues representing members of the Air Force, Army, Coast Guard, Navy, and Marine Corps. A 30-bell carillon plays each of the armed forces' hymns once a day. The other side of the monument has a fireman, policeman and emergency medical technicians rescuing a child. They are mounted on a platform about 2 feet above the levee walkway. They are mounted outside and in line with steel riverboat smokestack-like columns, which represent Lawrenceburg's Ohio River heritage. This is the largest monument of its kind in the United States.



The 45-foot high smokestack towers provide the framework that holds up two 12-foot-diameter clocks. One clock faces the city while the other faces the river. The carillon frames the towers and signals the time each hour by playing patriotic songs. In addition a 66-foot steel bridge spans the memorial walkway that passes through the levee to the river. At the center of the memorial the American flag flies high with the Indiana state flag beneath it.

THE WHITEWATER CANAL—ADDENDA

A short distance to the east of the monument the sign for the Whitewater Canal, which was once located by the canal basin, has been re-installed on the River Walk. It reads:

Whitewater Canal
Important waterway of pioneer commerce. Built 1836-47 from Lawrenceburg to Hagerstown. Used until 1860. Succeeded by Indianapolis and Cincinnati Railroad on towpath.



These pictures show the Hunt House when it was the Grand Hotel and as it is today as United Community Bank. Note that it wasn't the tallest building for long. A taller one was built beside it. Angular parking is still used in front of the building today. A plaque is on the corner of the Hunt House Hotel. Photos by Chuck Whiting



Top: Various branches of U. S. services are represented in the memorial. Photo by Chuck Whiting
Center: Chuck & Ann Whiting stand by the Whitewater Canal marker in Lawrenceburg.
Bottom: The Lawrenceburg river walk. Photos by Bob Schmidt

THE JESSE HUNT HOUSE

Indiana's first skyscraper was built in Lawrenceburg in 1819-1820 for Jesse Hunt, a tavern owner. In the 1885 *History of Dearborn and Ohio Counties* published by F. E. Weakley & Company of Chicago, the following paragraphs show how the people felt about the building at that time:

"What is known by the older resident of Lawrenceburgh as the Hunt Hotel, a large, three-story brick building on the corner of Walnut and High Street, was erected in 1819 or 1820, by Jesse Hunt, and is said to have been the first three-story brick structure erected in the State. This three-story house, it is stated by old settlers, struck the then primitive citizens with a kind of awe of curiosity and wonder. While the third story was being added, frequent remarks were made, like 'What in the world is Jesse Hunt going to do with them rooms way up there? A fellow would break his neck looking out of them windows,' etc."

THE WHITEWATER CANAL—ADDENDA

"Until within recent years [1885] there were several old territorial relics in possession of Maj. Anderson, formerly proprietor of the Anderson House (old Hunt Hotel, above referred to), which consisted of an antiquated looking, high desk, and a common table both very solidly and honestly made, both of which formed part of the furniture of the first land office established in the Northwest Territory. These articles did their duty both at Vincennes and Cincinnati, the late Peyton Symmes being their last user in Cincinnati ere the land office was removed to Chillicothe. The old desk and table then became the property of Gen. Harrison, and were saved out of the destruction by fire of the Harrison homestead at North Bend."

It always functioned as a hotel with various names throughout the years such as the Grand Hotel or the King Hotel. The original building stood on the corner. Later wings were added to it. More recently it was faced with stone. Over time the back of the building began falling in.

In early 2000 a 3-block-long plan for revitalizing High Street was put in place. By 2001 approximately \$6 million from the city and \$4 million from Historic Landmarks of Indiana, Inc. was spent on it.

In 2001 Lawrenceburg received a \$1 million ISTEA (Intermodal Surface Transportation Efficiency Act of 1991) grant from the Indiana Department of Transportation for rehabilitation of the eight East High street row houses and the Jesse Hunt House. They qualified for the funds since the buildings were near the Whitewater Canal terminus, the Ohio River and the Indianapolis and Cincinnati Railroad, which had its tracks laid in 1853 adjacent to the hotel where such notables as William Henry Harrison, Benjamin Harrison and Andrew Jackson once slept. Of these funds, the Hunt House received \$100,000 to complete the interior of the first floor. Within the past few years it has been restored and now houses United Community Bank.

OLD ELM ROW

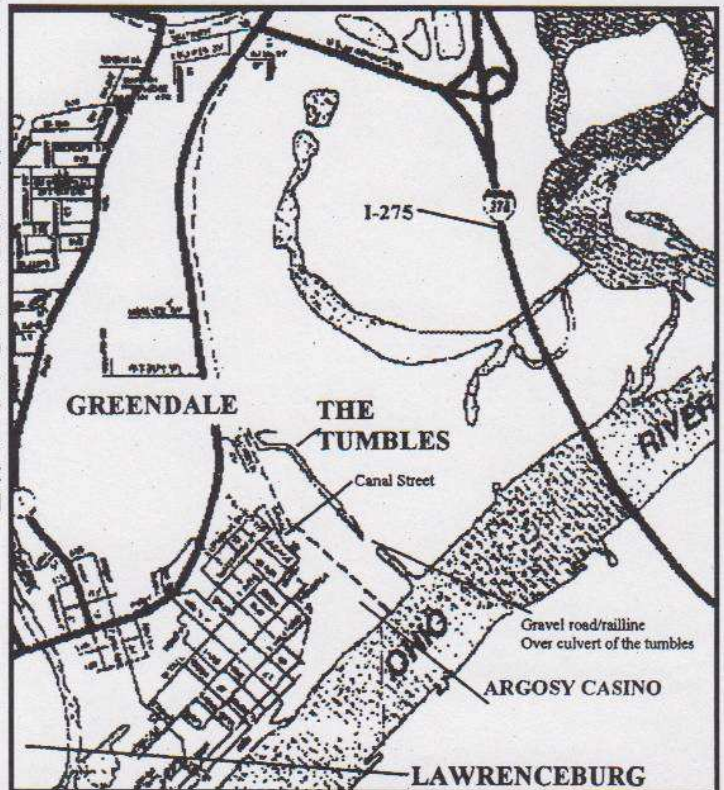


- 1820 Before the levee was built, Elm Row continued down the river to the city wharf — now under water.
- 1836 Indiana appropriates \$1,400,000 to begin construction on Whitewater Canal linking the National Rd to Ohio River
- 1837 The terminus of the canal is completed directly across from this plaque. E. D. Johns immediately builds Lawrenceburgh's first commercial flouring mill later called "The Old Water Mill."
- 1838 Renamed "The Miami Mills" (and distillery) it produced 300 barrels of flour per day.
- 1846 Violent floods damaged the canal—it begins to fall into disuse and railroads eventually buy rights to canal towpaths.
- 1848 Miami Mills machinery sold to the Gaff brothers of Aurora (Hillforest) for their distilling operation
- 1885 Mill used as sawmill by R. Duck. Later becomes apts.
- 1939 Current levee built after disastrous 1937 flood.
- 1999 Old Water Mill demolished

THE TUMBLES



The Lawrenceburg tumbles for the Whitewater Canal were only used when the basin at the foot of Elm Street was full of water. Excess canal water flowed out a waste weir located opposite the Dearborn County Fairgrounds into an outlet stream, which emptied into the Ohio River approximately one-half mile east of town.



**SPECIFICATION
FOR MASONRY
IN
CUT STONE LOCKS
ON THE
WHITE - WATER CANAL.**

QUARRYING AND CUTTING.

The face stone for locks will be prepared for regular ranges or courses of equal thickness throughout the whole length of the wall, and must be at least as much as Eight inches thick. Headers six feet long for the thicker, and five feet long for the thinner courses, and as much as 2 feet wide throughout their whole length, must be placed 8 feet apart in the clear in every course except the bottom and top courses. The stretchers will be from three to eight feet long and at least 18 inches broad throughout their whole length and must in no instance have less breadth than thickness. Each course will require four blocks of stone for hollow quoins at least three and a half feet long and three feet wide; the back part of which, as well as the face, must be square and true. The coping will be as much as 10 inches thick, three feet wide and in pieces not less than four feet long.

The Backing shall be prepared of sound and well shaped stone, their opposite beds being parallel. They should be at least 3 inches thick and they will be so selected that either one or two courses will raise to a height corresponding with the face stone. Backing headers or binders 4 feet long and 18 or 20 inches wide will be laid across the wall in the space between the front headers. About twice the number of these headers will be required for each course of backing that is called for in a course of face stone. No stone will be received for backing of less superficial area than 3 square feet, except such as are needed to fill the cavities that unavoidably occur between the larger stones.

The face of the front stone will be neatly dressed with the bush hammer, and great care must be taken to have the corners perfectly square. The beds must be dressed to a true and smooth surface in such manner that the stone shall not be any thinner at the back than at the face. The end joints will be so cut that when the stone are in contact at the face they shall not be an inch assunder twelve inches back.

LAYING LOCK WALLS.

The bed for the face stone will be prepared by spreading on a coating of soft mortar extending eight inches back from the front: the wall being wet before it receives the mortar. The stone to be laid will then be wet, brought to its position and settled with a heavy maul until the prominent points bear firmly on the stone beneath, crushing out all superfluous mortar and leaving only so much as will fill the cavities. The face of the stone will be brought to its vertical position by sinking and not by raising the back part. The ends of the stone will be placed closely in contact without mortar, and the joint will afterwards be filled by crowding the mortar forwards from the back of the stone by means of a thin iron blade made for that purpose. Headers shall be so placed in each course that the space between them shall not average more than eight feet; and their position shall be such as will bring them over the space between headers in the next course underneath. The face stones must break joints at least one foot.

The backing will be laid without mortar, and in such manner as to form the strongest bond that can be given to unhewn stone; and no small stone will be admitted except for filling up the cavities which unavoidably occur.

No stone shall be cut or hammered on the wall. After the backing of a course is completed, the joints in the back side of the wall will be plastered with coarse mortar, and the embankment and puddle raised within three inches of the top of the wall. The wall will then be completely filled with well wrought grout, and the front stone trimmed and made ready for the reception of another course. Immediately before letting in the grout the whole wall must be thoroughly wet with water. No masonry can be laid between the 10th of October and 1st of April.

The face stone will be laid in water lime mortar—extra cost of furnishing the lime over common lime being being paid by the state.

J. L. WILLIAMS, *Prin. Eng.*

Connersville, Nov. 14, 1838.

THE WHITEWATER CANAL—ADDENDA



WHITEWATER CANAL TRAIL

"Interest in placing a trail along the Whitewater River Valley dates as far back as the 1930's as a project of the Works Projects Administration (WPA). At that time, Eli Lilly himself was on the board involved in planning the trail, until the project became sidetracked in the chaos of WWII." (Whitewater Canal Trail website)

In the 1990s The Indiana Department of Natural Resources began a study for a trail from Metamora to Brookville and produced an elaborate Development Plan for the Whitewater Canal Trail. Jay Dishman, Curator of the Whitewater Canal State Historic Site at Metamora, and his crew worked on building the trail. Land acquisition problems eventually led to the abandonment of the project.

Over ten years the dream of the trail was dormant until Mick Wilz began talking to people and rekindling the idea. He organized the Whitewater Canal Trail, Inc. to secure volunteers and obtained over \$16,000 in funding from Visteon. His group partnered with the Indiana Department of Natural Resources in a cooperative arrangement on a section of land that runs along the canal. By April 30, 2005, the Yellow Bank Trailhead and 2-mile Yellow Bank Trail were completed and a ribbon cutting ceremony held. Later in the year an additional 1.5 miles of trail was completed from Metamora to the east. They received a \$150,000 Recreational Trails Program Grant from the Indiana Department of Natural Resources for improving the section of trail between Metamora and the Twin Locks. These sections were upgraded in 2006.

On the Whitewater Canal Trail website a description of the trail is as follows:

"The trail follows a former Big Four/Penn Central line and towpath which parallels the Whitewater Canal.

Several locks are visible from the trail, including a restored lock in Metamora. The scenic, historic area is a popular tourist & recreation destination, as well as home to the tourist-oriented Whitewater Valley Railroad. The charming small town of Metamora features a wealth of shops, festivals and attractions, including a working canal boat and the Metamora Grist Mill, which is the most visited historic site in Indiana."

The website also gives the purpose of the trail as follows:

"This 14-mile stretch of historical property tells the story of Franklin County's canal and trail history. It also showcases the beauty of the county's hills and river. By using this land for county citizens to exercise and enjoy nature, the history will be preserved and appreciated. By joining together, Franklin County can take advantage of this once-in-a-lifetime opportunity to clear the brush and bring a clear vision of progress to our trail."

Many volunteer hours have gone into developing and maintaining the trail and building kiosks at trail heads. This is just the beginning as the trail committee is working to open the trail completely from Brookville to Metamora. They also hope to eventually extend the trail to Laurel. And, if railroad property is abandoned, the trail could someday be extended southeast into Harrison, Ohio, and the Cincinnati area.

There are still some land issues to be resolved before the entire trail can be completed. Hopefully, as land owners see how well the trail is kept up and how responsible people are who use the trail, the land owners will agree to let the trail run through their property.

For time being, if you want to leisurely stroll along the old Whitewater Canal, the trail can be accessed at the Metamora Trailhead or the Yellow Bank Trailhead. Directions to them are as follows:

Metamora Trailhead: From US52 at Metamora, take Columbia Street south. Cross RR tracks; turn left (east) on Main Street. Parking is located at the trailhead where Main Street ends.

Yellow Bank Trailhead: Follow US52 east of Metamora for about 3 miles. Trailhead is on the south side of the highway at the Yellow Bank Road intersection.

To join in this effort contact Whitewater Canal Committee, P.O. Box 126, Brookville, Indiana 47012 or visit their website at:

<http://www.whitewatercanaltrail.com/>

THE WHITEWATER CANAL

ARTICLES FROM THE BROOKVILLE AMERICAN ON THE WHITEWATER CANAL

Also Cincinnati and Whitewater Canal, East Fork Canal, Wabash & Erie Canal, and Central Canal

(The paper often reprinted articles from other newspapers. Their names are noted with each article.)

Compiled by Stan Schmitt

Typed chronologically by Carolyn Schmidt

1836

2-26-1836

Ohio vs. White Water Canal. - We hear nothing from Columbus, suspicious of their granting us right of way. Geo. H. Dunn, Esq. we understand, is now at Columbus, pressing our petition. He writes that the prospect is any thing but favorable. For the honor of that proud state we hope they will not carry their narrow contracted selfishness so far as to render themselves contemptible in the opinion of all honorable men, and to be ridiculed by all civilized nations.

3-18-1836

Board of Internal Improvement - This body convened at Indianapolis on last Monday week, but having received no papers from Indianapolis for about a month, (either ourself, or subscribers to the Indianapolis papers,) we can give nothing of their proceedings, further than we learn by a letter from a distinguished citizen of Indianapolis to a gentlemen of this place. The Board have instructed the Fund Commissioners to leave immediately for the eastern cities to negotiate a loan. They have also ordered that the Whitewater Canal be permanently located immediately, and that part from Brookville to Lawrenceburgh, to be put out on contract for construction as soon as the preliminary arrangements can be made, which will be some time the ensuing summer. This news is cheering to our prospects of a speedy, completion of this grand enterprise! The Board had the law of Ohio, granting Indiana right of way for the W. W. Canal before them, and are of the opinion that its provisions can easily be complied with, and are to make arrangements accordingly.

4-22-1836

BRANCH CANAL OR RAILROAD.

No doubt now remains, in regard to the making of the White-water Canal, and the interest of the State and Brookville require that a branch should be carried up the east fork as high at least as Richmond.

Some difference of opinion will exist whether this branch shall be by Canal, Railway or Turnpike. Either would bring the business to the main Canal and the question must turn on other considerations. If the east fork shall be found to afford water sufficient for the Canal and for the machinery which the country will require, all must ad-

mit that a canal is the proper work to make, as it will require no changes from one mode of transport to another. If there should be water enough for the Canal, but not for machinery; then the good sense of the community must determine whether, changing the property at Brookville from a car or wagon to a boat, will be a greater inconvenience, than the loss of its manufacturing privileges. If there shall not be water for the canal or it should be determined that the water power must be retained, then the question will be between the Rail Road and turnpike. The expense of making will not be essentially different. But the cost of transport upon the one will be 3 times that on the other, and a like, if not greater difference in time. Another consideration will be, that a rail way may be continued from Brookville to the Lawrenceburgh Rail Road and thus a continuous communication will be had from Richmond to Lawrenceburgh and from there to Cincinnati, as the Rail Road is to be continued to the latter place—at all times when the Canal is navigable the heavy products of the country will take that direction, when it is not, the country will have the benefit of the other. To personal travel the improvement is all important as the wants of the community in this respect can never be supplied by the Canal, the navigation of which must be necessarily very slow and especially on the Whitewater, where there is such an amount of lockage.

It is not to be expected that the State will make any more State works in the White Water valley, at least for several years. But her interest in such an improvement as the one spoken of, would be such as to justify her extending her credit to aid individuals in the undertaking. This matter therefore cannot be too soon brought before the people of the White Water. Arrangements should be made, so that the branch can be begun as soon as the main canal is completed to Brookville. Any man living on the line from Richmond to Lawrenceburgh is interested and should be consulted. Can this be better done than by having some central place selected and a day appointed for a public meeting during the coming Summer. WHITE WATER

4-22-1836

WHITE WATER CANAL.

The Legislature of Ohio has finally passed the bill authorizing Indiana to construct that portion of the W. W. Canal, which lies in the Territory of Ohio. The following is a copy of the bill, which was forwarded to us by Geo. H. Dunn Esq. from Columbus, Ohio. A BILL.

To permit and authorize the State of Indiana to construct a part of the Whitewater Canal within the territory of Ohio.

Sec. 1 Be it enacted by the General Assembly of the State of Ohio, That the State of Indiana be, and is hereby permitted to construct so much of the Whitewater canal

in the State of Ohio, as may be found necessary for the purpose of terminating said canal at or near Lawrenceburgh, in the State of Indiana: *Provided*, Said canal be located and constructed on such plan as will, in the opinion of the acting commissioner of the Miami canal, afford every reasonable facility to this State, to connect with and extend a branch canal from said Whitewater canal, to the lower plane of the city of Cincinnati: and that the said branch shall be supplied from said canal with all the water that shall be necessary for the navigation of the branch.

Provided also, That previous to the final location of the canal, a survey shall be made on each side of the White Water from Brookville to the mouth of said river and also west of the village of Elizabethtown, and in determining the location and plan of the Canal, reference of the neighborhoods through which said canal may pass so far as may be practicable and expedient without increasing materially the cost of the work.

Sec. 2. That all the laws now in force, or which my hereafter be enacted for the construction and protection of Ohio canals, and the works connected therewith be and are hereby extended in the construction and protection of so much of said Whitewater canal, and the necessary works connected therewith, as may be located in the State of Ohio.

Sec. 3. That the said State of Indiana shall have right of collecting tolls on said part of the Whitewater canal, at the same rates which may be charged on said canal in other parts of it: *Provided*, That no greater rates of toll shall be charged on property passing on the said Whitewater Canal, which (property) may pass on any branch of said canal that may be made by the State of Ohio, that is charged on property passing entirely on the said Whitewater canal.

Sec. 4. That all claims for damages., on account of injuries sustained by private individuals for the construction of, or in the repairing and securing said canal with the State, shall be submitted by the proper officer of the Whitewater canal, to the acting commissioner on the Miami canal; whose duty it shall be to appoint a board of appraisers, consisting either of the board of appraisers for the Miami canal, or of other three disinterested freeholders, citizens of this State, which board, or a majority of them, shall examine and decide on all its claims, when submitted to them by the said acting commissioner, in accordance with and agreeably to the laws of this State, relating to assessments of damages on the Ohio canals, and report the same to the proper authority of Indiana; which assessment so made, shall be paid by the State of Indiana, and shall be final: *Provided*, That the appraisers shall be paid for the services by the State of Indiana, at the rate that the

THE WHITEWATER CANAL

appraisers, for similar purpose, are paid in this State.

Sec. 5. That whenever, in the construction of said part of said canal, it shall be necessary to cross any road or way, it shall be the duty of the State of Indiana, so to construct the said canal, as not to impede the passage of transportation of persons or property along the same; and shall construct good and substantial bridges over said part of said canal for said roads, and shall keep the same in repair, and reconstruct the same when necessary.

5-20-1836

W. W. Canal. - The Engineers on this work having gone through with a preparatory survey, commenced its final location on Tuesday morning last, beginning at the feeder dam above this place. It is yet undecided, which side of the river the Canal will be located below Brookville, but we presume it will be known before the publication of our next paper.

The Canal Fund Commissioners have not been heard from, we understand, since their departure for the Eastern cities, but it is supposed by those acquainted with the "money markets" that a loan will be easily obtained. A small accommodation has been effected at the Evansville Branch of the State Bank, to pay the current expenses of surveying and locating the different works in the State.

5-20-1836

Internal Improvement Celebration. - The citizens of Evansville are to have a great Internal Improvement celebration of the 4th day of May next, on which occasion it is expected the Governor, Lieut. Governor, and several members of the late Legislature will be present, as invited guests. We have received an invitation to participate in the festivals of the day, and take this method of conveying to the committee the assurance of a lively sensibility on our part of the honor conferred, and of tendering to them our best wishes for the success of the great system, which has called forth this celebration. However, much we might be gratified with an opportunity to partake of their kind hospitality, in common with others of our co-laborers of the last Legislature,--however cheering it would be to our feelings to meet them around the festive board, the nature of our engagements are such, that we are bound to forego the pleasure. We wish them a heavy turn out, good cheer, a pleasant day and a happy termination of it. -Ind. Palladium.

5-27-1836

CANAL CELEBRATION AT EVANSVILLE. - On Wednesday of last week (being the 4th of May) the citizens of Evansville celebrated the passage of the Internal Improvement Bill with great liberality. About 300 individuals sat down to a dinner provided for the occa-

sion, at which good cheer was provided and loud cheers heard during the day. About twenty members of the last Legislature were present (as invited guests) together with the Lieutenant Governor, some of the Board of Internal Improvement, and an Editor whose name we don't like to mention. Though the day was somewhat disagreeable out of doors, it was very far from being so under the hospitable roof of the citizens of Evansville. If the most delicious viands, sparkling champagne, and sparkling speeches, could add to the joy of the friends of the "great bill," then all must have been Happy. LEUTENANT GOVERNOR WALLACE on being toasted, addressed the assembly in a powerful and eloquent speech, during the delivery of which he was often interrupted by "loud and long" rounds of approbation. MILTON STAPP, of Madison, followed, in reply to a compliment from the company, and was, as usual, both eloquent, pungent, and witty during his speech. Col. T. J. EVANS (the chairman of the committee of Internal Improvement,) also rose to respond to a similar toast in reference to himself, which he did in a very neat, feeling, and appropriate manner. The late Representative of Vigo, Col. E. M. HUNTINGTON, was next called up by a sentiment complimentary both to himself and our own county, and he also addressed the company in a speech of great length and power, during the delivery of which he spoke in the midst of deafening applause. -Wabash Cour.

5-30-1836

Branch Canal or Rail Road - We call the attention to our readers, and especially our brethren of the Press in Lawrenceburgh, Liberty, and Richmond, to a communication in this paper, upon the subject of some public improvement between Brookville and Richmond. The author of this communication has well matured the subject of Internal Improvement, in his own mind, and has been one of the principal individuals in this State, who have been instrumental in forming public opinion, in favor of our glorious system of Internal Improvement. He advocated the cause when there were few to join with him; and contended for particular works, when by the majority it was considered visionary to think of such things for forty years to come. But those works now are in a fair prospect of being finished in a few years. The Lawrenceburgh and Indianapolis Rail Road will be finished in a few years by individual enterprise, and the same master spirits who have overcome all opposition to this work, are interested in the proposed work to Richmond; and with the aid of the wealthy and enterprising citizens of Richmond, Liberty and Brookville, and the intermediate country, who cannot foresee the day when this work will be finished. We hope our brethren of the Press, interested in this work, will aid in bringing this

subject before the public, and also, propose a place of meeting.

6-3-1836

Internal Improvement Celebration. - We have received the proceedings of the great Internal Improvement Celebration which was had at Evansville, Ind., the first of this month, and shall endeavor to lay a synopsis of it before our readers. It was a celebration in honor of the able advocates of the Bill of Improvement in the last Legislature. Deep and exciting interest was felt on the occasion, and called forth sentiments worthy of patriots. We think it will be interesting to our readers.

6-10-1836

From the Centreville Advocate.
WHITE WATER CANAL.

Through the politeness of Gen. E. Long, one of the members of the State Board of Internal Improvements, we are enabled to lay before our readers a communication on the subject of the White Water Canal, in the early commencement and construction of which the people of this section of the State are so deeply interested.

The friends of improvement will be pleased to learn from such a source that the Superintendent has prevailed upon the State Board to assent to the immediate location of the whole line, as far up the White Water Valley as the same may be found practicable, and that he has so promptly determined to keep the corps of Engineers under his command, in the field until the same is finished, and that consequently we may confidently expect the whole location to be permanently completed the present season. By this means the contemplated improvements along the line will be, we think, greatly facilitated, sites for machinery developed, towns and villages rise up, and a market for our produce be created at once. First by the consumption of the laborers employed at the work, and forever after its completion boat warehouses and depots to be established on the bank of the Canal.

6-7-1836

Dear Sir:--You are, no doubt, apprized of the great anxiety of the citizens in the vicinity of the proposed White Water Canal, to have the line of the same permanently located as soon as convenient; and believing, as I do, that you are at all times anxious to give the citizens any information in your power, I have taken the earliest opportunity to inform you that the Board Of Internal Improvement at their recent meeting at Indianapolis, gave me permission, after completing the location of that part of the Canal ordered first to be put under contract that is South, or below the first feeder dam above Brookville, to cause the line of Canal to be re-surveyed with a view of permanently locating as far North as practicable, as soon as possible. -The party of Engineers that is

THE WHITEWATER CANAL

engaged with me, will probably complete the location that they are now engaged in, in less than four weeks, and will immediately commence North. I have no doubt of the propriety of locating all of the Public Works as soon as practicable. We shall not be able to get the services of competent Engineers at salaries as low as was expected. This is caused by the whole country being engaged in Public Works at the same time, and the scarcity of experienced Engineers in our country. I have no information from the Fund Commissioners worth your attention, when any thing is received you will hear from me. Very respectfully, E. LONG. S. C. MEREDITH.

8-26-1836

THE CELEBRATION - The preparations are still progressing for the proposed celebration on the 13th day of September next, in this place. If the weather should be favorable, we have no doubt it will be one of the largest assemblages ever convened in Indiana. The committee of invitation has received answers from many of the distinguished individuals, advising the committee that they will attend this festival. Lieut. Gov. DAVID WALLACE, was selected by the Committee of arrangements to deliver an address on the day, and he has accepted the appointment. Letters and information have been received from every direction, giving assurance that, if the weather permits, a vast multitude will grace the celebration.

THE CANAL - We learn from the Engineers that the Canal from this place to Lawrenceburgh, has been permanently located, and staked out, ready for the contractors. They can now examine the different sections and prepare for the bids. They will recollect that next Tuesday two weeks is the day for contracting.

9-2-1836

CANAL CELEBRATION, At Brookville on the 13th day of September. **ORDER OF THE DAY.**

The procession will be formed under the direction of the Marshals, on or near the public square, at precisely 10 o'clock A.M. and March to the stand erected for the purpose, where the Oration will be delivered at half after 10. Then the ceremony of "breaking the ground" for the commencement of the White Water Canal will be performed by those selected, by the committee of arrangements, for that purpose. The dinner will be served up at precisely 12 o'clock M.

ORDER OF THE PROCESSION

1st Marshall and Assistant Marshall, (mounted.)

2d. Music and Military.

3d. Orator and President of the day.

4th Vice Presidents of the day, (two & two.)

5th. The canal commissioners & engineers, (two and two.)

6th The gentlemen who are to perform the ceremony of breaking ground, (two and two,) with all the necessary implements.

7th. Invited guests, and strangers from abroad.

8th Governors,--heads of department,--members of Congress, and of the State Legislature, and Judiciary officers, (two and two.)

9th. Committee of arrangements, and other committees (two and two.)

10th. Citizens, (two and two.)

Two assistant marshals, (mounted.)

9-9-1836

EAST FORK CANAL.

The Delegates to the Dunlapville Convention will recollect that next Monday is the day. The importance of the subject and trust reposed in you by your fellow-citizens as Delegates, should induce you to forego all other considerations to attend the day specified. Delegates have been appointed from Richmond, Abington, Brownsville, Dunlapville, Fairfield, and Brookville. When they meet on Monday next, we think it would be well for them to deliberate and advise with each other upon the different advantages and disadvantages between a Rail Road and a Canal, in connection with the situation, wants and conveniences of the country. We are not certain but a Rail Road would be preferable, all things considered.

But one thing is certain, either one or the other can be done --and must be done. The circumstances and ability of the citizens demand it, nor will we cease our cry until it is done. We hope the Convention on Monday will speak in a voice worthy of those who have delegated them. The following is a list of the Delegates from the different towns, also a list of the Delegates to the Brookville Celebration:

From Richmond -- Jas W. Borden, Achilles Williams, John Finley, W.M. Leeds, John Ewing, Oliver Kinsey, J.R. Mendenhall, Benjamin Sayre, Caleb Shearon, Stephen G. Stout, Wm. R. Foulke, Alexander Stokes, and Irvin Reed. --These were also appointed delegates to the Brookville celebration, to be held on next Tuesday.

From Brownsville. --William J. Bennett, Esq. Wm Watt, Esq. James Lamb, Esq., Jesse Starr, Esq., Wm. Youse, Thos, K. Hardin, J.F. Chapman, Ladus Walling, Jacob Imel and Richard Dormire.

John Yarman, Esq., Jesse Starr, Esq., Wm. H. Bennett, Esq., Jacob Imel, and Wm. Youse, were appointed from Brownsville to the Brookville Celebration.

From Brookville. --Abner M'Carty, Samuel Goodwin, J.M. Johnson, George W. Kimble, Geo. Holland, Robert John, and John Ryman.

From Fairfield. -- Christopher Marters, Daniel Ogden, Redin Osborn, James Wright, and Levi Munson.

From Abington. --Col. Smith Hunt, Thos.

J. Larsh, W. Matchett, Harland Robins, Jacob Fender, John Whiting, F. Black, and Jacob Plessenger. The same were also appointed Delegates to the Brookville Celebration.

From Dunlapville. -- Geo. Newland, John Templeton, J.W. Scott, Matthew Hughes, and Hugh M'Culloch. The following committee was also appointed to attend the Brookville celebration - to wit: Mark Test, John Templeton, S.C. Swan, Wm H. Mosby, Bennett Osborn, and Col. Abel Abernathy.

9-30-1836

WHAT'S THE MATTER. - We see they have held a public meeting in the White Water Township in Hamilton Co., Ohio, upon the subject of the White Water Canal, at which meeting it was resolved that the Commissioners, or Engineers, who located the White Water Canal, have not located the line agreeably to the letter, and spirit of the Act granting Indiana the privilege of constructing the Canal through a portion of Ohio. We wish it's meting would be a little more specific in their objections to the location of the Canal. The law does not make it obligatory for us to go round by Cincinnati or by North Bend to get to Lawrenceburgh.

9-30-1836

GOOD NEWS FOR INDIANA - Success to her - She deserves it. The last New York Star has the following paragraphs. - From the same source we learn how a corporation dinner may be got up; it is an apt illustration of patriotism of the modern school - it follows.

Morris and Canal Banking Company. - We understand that this canal & Banking Company, have taken the loan of *Two million of dollars* for the State of Indiana at five per cent loan for the Internal Improvement of that new and rich western State. The Directors of this company deserve the cordial thanks of the public for the exertions they have made in strengthening the cause of Internal Improvement while at the time their able and judicious arrangements, abroad, they will be enabled hereafter to facilitate the commercial and moneyed interest of this city, to a greater extent than any Banking institution of this country.

10-14-1836

EAST FORK CANAL. *From the Richmond Palladium.*

Mr. Editor,--It is with pleasure I have viewed the interest which the people take in the proposed Canal between Richmond and Brookville. In examining all the proposed improvements, I have come clearly to the conclusion that this is the only available project and I likewise have no doubt of its being the most advantageous improvement possible for us to make. This work would be of vast importance to the farming interest as well as the towns located on the

THE WHITEWATER CANAL

route, by bringing the best of markets to all of our doors in White Water valley and vicinity, without injuring any class of citizens.

I have heard it said, by some, that the water was not sufficient; the water has been three times measured by competent judges and supposed to be plenty to answer well.

After conversing much with experienced individuals, who have spent years in canalling, I am still of the first opinion I before advanced, that the cost of this project would be about \$300,000. Some say that a single lock will cost \$10,000, this idea may do where stone is far to haul. Engineers say that the cost of a lock in the immediate neighborhood of rock quarries is generally estimated at \$5,000. And such is the fact, that at every point where a lock, a protection wall, or culvert is needed on this route, stone in abundance is hard by.

Some again say there is so much fall in our river the lockage would be too costly an undertaking. What are the facts? I suppose there is not more fall on this route than on the west fork route, and if it is, what of it? Our most experienced mechanics, who have been for years engaged in machinery propelled by canal water power at the locks, say, that the water created at each lock is worth as much the cost of the lock to the man who improves said power. I would like for some of our wiseacres to say how much those mills and machines would be worth to the neighborhoods where located. Nor need the mills be spoiled by this improvement, because with the exception of about six or eight weeks in the year there is water enough for both canal and mills, and these few weeks happen at the time when there is the least produce to send off, consequently the mills now running might have the use of the water to a very considerable amount without interfering with the canal. M

10-21-1836

CANAL LETTING.

All the sections on the Central Canal were let out at fair prices, rather below the estimate, with the exception of sections 32, 36 & 40. The bids on these sections were so high, that the Board have not as yet awarded them to any one:

A LIST OF CONTRACTORS

Section No. 1, Burk & Ferree; 2, Geryer, Young & c.; 3, James M'Clure; 4, Ferguson & Kimberlin; 5, Edward Penderville; 6, Divine & Sheridan; 7, Thomas Fitzgibbon & co.; 8, James N. Pope; 9, T.J. Matlock; 10, Jas Sheehan & co.; 11, T.J. Matlock; 12, S. and C. Connelly; 13, Cassett and Barnett; 14, Barcus and Kinirk; 15, Geo W. Stipp; 16, Jno. S. Matlock & co.; 17, Z. W. & C. Sutherland; 19, Elder and Smith; 20, Ferree and Adams; 21, J. Berlin & co.; 22, A. Murphy & co.; 23, Andrew Wilson; 24, R. Colvin; 25, J.B. Matlock; 26, Robert Potter; 27, J. Herrin & co.; 28, Drillard and Mur-

phy; 29, Thos. Moore & co.; 30, Jno. S. Matlock & co.; 31, Jas. Shehan & co.; 33, Naylor and Alcorn; 34, J. D. Farrell & co.; 35, Nichols and Matlock; 37, Naylor and Hiatt; 38, John Burk; 39, Schofield & co.; 41, Jas. B. Matlock; 42, Jno. D. Kuntz; 43, A. Little; 44, John P. Terry; 45, James Smith; 46, John P. Terry. *Ind. Dem.*

[Note that Section 18 was not given in the article]

11-25-1836

CELEBRATION. - Our citizens propose having another internal improvement celebration on the occasion of letting the canal contracts for this part of the route.

The arrangements are not yet made in full; governor Noble, lieutenant governor Wallace, and the canal commissioners and engineers will be invited to partake of a public dinner; and we hope to see our governor exhibit as much grace or physical power in commencing the great central canal he did of moral strength and wisdom of promoting the adoption of the great system of internal improvement. - *Evansville Journal.*

1837

11-17-1837

THE CANAL -The enterprising contractors, J. Wilcox & Co. have finished the Canal dam across the East Fork of White-Water, at this place, and the water was let into about one mile of the Canal on Saturday last, and many of our citizens have had the pleasure of being first, (not only to advocate,) but of floating upon a portion of Indiana's Great Works of Improvement. The dam backs the water nearly up to Speer's mills, making the East Fork a splendid basin for the transaction of business. The sight of this small finished portion of the "mammoth system," calls to our imagination a picture of wealth, prosperity, and independence, of the White-Water Valley 50 years hence. It animates the heretofore drooping spirits of our citizens, and is to be discovered in their sparkling eyes, their quick and elastic step, and in their disposition to improve every moment to turn to profit the advantages they are receiving from the State.

We are assure by the Canal Commissioner, and by the resident Engineer, that the Canal from here to Lawrenceburgh will be ready for navigation early next fall. We have had a pleasant season for work, and it has been well improved by the contractors. We believe preparations are on foot for building canal boats next spring and summer, to be ready for the fall trade. It would appear that not only our citizens, but our canal contractors, are in favor of putting our canal in the "first class."

12-22-1837

THE CANAL -HIGH WATERS .-A VERY HEAVY RAIN FELL ON Saturday and Sunday last, and the ground being frozen, it swelled the White-Waters higher than they have been for some years. The Canal at the Forks of the River sustained considerable damage. It broke the embankment of the canal near the dam, on the north side, and carried away near 4,000 yards of embankment. We are informed by the Engineer that the damage will not, upon the largest calculation, exceed 1,000 dollars. Had the rise in the water been three weeks later, every thing would have been safe. But under the circumstance, the public have no reason to complain of those who have charge of the work. Every exertion was made by the contractors, engineers, workmen and citizens to save the canal. Notwithstanding the rain fell in torrents all of Saturday night, the contractors, Engineers, laborers, &c. continued on the embankment all night, to stop leaks, and raise the embankment where needed. Here, where every citizen saw the exertions which were made, no blame is attached to any individual for neglect of duty. The Dam is perfectly safe.

12-29-1837

CANAL, ENGINEERING, &c. - Last week, in the discharge of our editorial labors, we gave some notice of the damage done the canal near this place by the high waters. In order to be correct upon this subject, we obtained from the Engineers a true statement of the damage from actual measurement and estimate. But this *freshet* has graduated more Engineers than have been graduated at the West Point Academy since the days of Washington; and we are reminded, at every corner of the street of the anecdote of the old woman and the cow eating the grindstone; "*I told you so!*" Those who are always hunting up objections to our Internal Improvements, have taken particular exceptions to our statement of the amount of damages. Some, possessing more *impudence* than *common sense* or *common courtesy*, have boldly charged us with making *false statements*. With such, we have nothing to do. We stand on an elevation far above the reach of these pretended knowing ones, who are as ignorant of the value of labor, and all kinds of business, as they are of the track of the Comet Encke. Our business is to record faithfully, regardless of the prejudice of the ignorant, or the schemes of the designing.

1838

2-2-1838

NOTICE TO CANAL CONTRACTORS.

The Cincinnati and White Water Canal Company will receive proposals at their office on Third Street, on the 28th day of February next, for the works required to pass the Canal over Mill creek, Bold-face, Rapid-

THE WHITEWATER CANAL

run, Muddy, creek, the Great Miami River near Cleves, and the Dryfork of the White Water; also for the construction of a guard and lift lock near Dryfork Aqueduct, 8 feet lift; a lock of 5 feet lift near the east end of the Miami Aqueduct, and for the deep cut excavation, and the construction of a *tunnel* through the ridge which separates the valleys of the Great Miami and Ohio rivers near North Bend.

The work to be performed at the crossing of the mouths of the creeks emptying directly into the Ohio river, will consist of stone arches of from 30 to 50 feet chord, with heavy embankments over and connected therewith; that as the crossing of the Miami and Dryfork will consist of Aqueducts, the abutments and pieces of which to be built of stone, and the trunks with timber, and of heavy embankments in connection with the Aqueducts; the work at the North Bend ridge will consist of very heavy deep cutting through the ridge, and a stone arch, laid in this cut, of 24 feet chord, and about 24 chains in length, resting on abutments of ___ feet in height, which will be covered with the excavation as the work progresses; thus, when the work is completed, making a tunnel through the ridge.

Contractors are invited to examine these different items of work with care; they are of a heavy and responsible character well worth the attention of men of experience as contractors. Information vouching the peculiarities of the work to be let, may be had by applying to D. Lapham, the Engineer in charge of the work.

Plans and specifications may be seen at the office of the Company ten days prior to the day for receiving proposals.

By order of the Board of Directors.
M. T. WILLIAMS, President.
Cincinnati, January 24, 1838.

2-23-1838

CANAL LETTING. - We learn from Gen. Long, Canal Commissioner, that before the adjournment of the board of Internal Improvement, he was authorized, so soon as favorable information be obtained from the Fund Commissioners, to advertise and let the contracts for the White-Water canal to where he was first authorized, to-wit to the first feeder dam above Brookville, which is about five miles above this place. He is also authorized to let heavy contracts between that point and the National Road to the amount of \$300,000. There is little doubt but the letting will be early in the Spring, thus giving every assurance to those on the live above this that the work will be prosecuted by completion as soon as possible.

4-7-1838

SALE OF TOWN LOTS In the Town of METAMORA Franklin County Indiana

THIS subscriber will offer at public sale on Thursday the 26th of April, A. D. 1838,

about fifty in-lots, in the above named town. -This place is situated in one of the most extensive, fertile and beautiful valleys of the white water river. It is nine miles from Brookville, twenty-one from Rushville, twenty-three from Greensburgh, fifty from Cincinnati and sixty from Indianapolis. The white water canal is located directly through the center of the town, and the construction of the work will be commenced early in the summer. -There will be one lock in the town and another within a half mile, of nine feet lift, creating water power sufficient for twelve run of mill stones. -In addition to this the white water river affords ample water power at all seasons of the year, and there are now in operation upon it, in sight of the town, a grist mill, saw mill and carding machine.

The great State Road and stage route from Indianapolis through Rushville and Brookville to Cincinnati, strikes the canal at this point, and runs side and side with it through the center of the town and nearly all of the lots offered will front on one or the other of these great thoroughfares. The road from Connersville through Laurel to Brookville unites here with the great state road, and also the road from Greensburgh by the way of Clarksburgh, enters the white water valley at this point. In addition to those the roads down duck creek, pipe creek and salt creek, which all empty into the river near this place afford the most natural channels for transporting the produce of the extensive country along those streams.

There is no point on the white water canal where a greater amount of produce will be taken to and from the Ohio river; and the number of hogs which will be driven here, must, owing to the great concentration of roads at this point, from the rich country west, always greatly exceed the number taken to any other point on the canal. Barrels and kegs for packing pork and lard can be made very cheap here owing to the abundance of materials for that purpose. The soil, of the valley in which Metamora is situated, is a rich dry loam, without any stagnated water in the neighborhood. Water of the purest kind is obtained in wells of moderate depth, and the health of the place and vicinity is unsurpassed by any place in the west. Timber, stone and materials for brick and lime are convenient and in the greatest abundance.

Such has been the demand for a place of business at this point, that though no town has heretofore been laid off, a store, tannery, post office, and numerous mechanic shops have been established and are doing an excellent business, and the proprietors in laying out this town, believe they are merely answering the wishes and wants of the rich and rapidly improving country around it.

The terms of sale will be one third in six months, one third in twelve months, and

the balance eighteen months without interest, the purchaser giving his note with approved security.

DAVID MOUNT, WILLIAM HOLLAND

P. S. Mr. Holland one of the proprietors will offer at the same time and place, and on the same terms, three forty acre lots of land in sec. No. 30 town. 12 R 13., 35 acres in fractional sec. No. 10, town. 10 R 11; also the west half of the north west quarter of sec. 6, town. 11, R. 13, containing 52 acres, and the north east quarter of the north east quarter of section No. 11, town. 11, R. 12, containing 40 acres.

W. HOLLAND

March 23—tsThe Cincinnati Gazette will publish the above till sale and charge American office.

8-3-1838

WHITEWATER CANAL - A canal letting took place at Connersville, on the 24th alt. for 31 additional sections of the Whitewater Canal. It consisted of five miles from this place north, and the heavy sections, dams, locks, and culverts, from the termination of the five miles to Cambridge City. There were a large number of bidders, and the sections were let on very reasonable terms. The following is a list of contractors, and the number of the section awarded to each:

No.

SECTIONS CONTRACTORS

62	George Heimer,
63	Reid and Kelly,
64	Wm. Marshall,
65	Wm. Griffin & Co.,
66	Putney and Russel,
67	L. Laughlin,
68	George Heimer,
69	Perrine, Kyle & Co.
70	N. Hammond,
71	M. Garrison,
72	Sopris and Ballenger,
74	Carmichael & Barwick,
75	Wm. M. McCarty,
78	Rubottom & Westerfield,
79	Isaac Van Horn,
80	Gibbon and Williams,
84	Halstead and Parker,
85	H. Simonton,
86	George Heimer,
90	J. Palmer,
91	Naylor, Troxell & Co.,
93	Branham & Co.,
104	Scott and Butt,
112	(withheld for future advisement.)
113	Butt, Corbey & Dunohue,
114	H. Lasure & Co.
115	(withheld for future advisement.)
117	Vance, Coldwell & Co.
119	Vance, Coldwell & Co.
132	Tyner, Whipple, & Co.
133	C. & J. Meek,
137	Rubottom & Westerfield.

THE WHITEWATER CANAL

9-21-1838

Canal Commissioner.—We are pleased to learn that the Governor has appointed A.F. Morrison, Esq. as Canal Commissioner, in the place of Daniel Yandis, resigned. From a limited knowledge of Mr. Morrison, we believe it to be a good appointment. He was formerly editor of the Indiana Democrat.

CENTRAL CANAL—We have the satisfaction to be able to state, that the portion of some twenty-five miles of the canal leading through Indianapolis is nearly completed. —Some heavy jobs will not be finished until sometime in next month, but the latter part of which it is supposed it will be ready for letting in the water. Much credit is due to the contractors for their efficient manners of carrying on the work. The lock at this place is nearly completed and will be a specimen of architecture that will well compare with the best of the New York Canals. We notice one improvement of no small consequence, and speed to the passage of boats. The upper gates are of the same depth as the lower ones; the abutment being placed back far enough for the gates to open. The paddles in the gates for filling the lock being near the bottom, the water is thus let in number (under) and raises it, without the inconvenience of water's pouring upon the bow, as is the case when the upper gates are placed upon the abutment, and the lock is sooner filled, in consequence of the great head of water above the paddle gates.

Much benefit has resulted to our community, during the two years this work has been in progress. Our farmers have had a ready market for their produce, and our merchants and mechanics have received no small addition to their business. It has been a time of great pecuniary pressure, but the spending of so much money in our vicinity has much alleviated the distress, that otherwise must have borne much more heavily upon our citizens. Although we have yet no outlet, but the part finished will immediately be of use. By its completion we have a fine water power added to our other advantages, and machinery will soon be put in motion, and a market at once created for the raw material for the purpose of manufacture. A considerable business will be done in the conveyance of heavy materials to our market, such as wood, lumber, stone, lime, &c. The building of boats, a new branch of business, will be commenced, and various other kinds of business not now carried on here, will soon go into operation; consequently the number of inhabitants will increase, and the demand for the farmers produce will continue, for home supply. By the time our farmers get fairly at work on the improved system of farming, by which they will have larger quantities of produce to spare, Lake Erie will be open to us on the north and the Ohio on the south, —and a choice of markets will be brought to our door.

Such is a picture of the benefits we may expect to result in consequence of the completion of our canals and rail roads, not only to our section but to all portions of the state. *Indiana Farmer*

10-5-1838

White-Water Canal Letting.

SEALED proposals will be received by the undersigned at Connersville, on the 14th day of November next. For the construction of all that part of the White Water canal, now under contract, between Brookville and the feeder dam at Somerset, together with several heavy sections between Somerset and Cambridge City. The work to be let will be divided into sections of suitable length, and will be staked out, and ready for the inspection of bidders ten days previous to the day of letting; and the plans, profiles and estimates of quantities will be exhibited at the place of letting three days immediately previous to the day of receiving bids.

Bidders who are not personally known to the Commissioner or Engineer, will be expected to present, with their proposals, satisfactory testimonials as to their qualifications and character.

ELISHA LONG, Acting Commissioner, W. W. Canal Sept. 25th, 1838 4--t

10-19-1838

White-Water Canal.—The past season has been peculiarly favorable for the prosecution of the public works in Indiana, and particularly for building dams, abutments for bridges, aqueducts, &c. the streams being nearly dried up. All the dams on the canal between Brookville and Lawrenceburgh were finished some days since, and the water will be let into this end of the line in a few days, and it is calculated that it will be let in, all the way from this place to Lawrenceburgh by the 20th of next month. We yet hope to have the pleasure of visiting our friends at Lawrenceburgh by the canal, before it is closed by ice, if a boat can be had. Who are building boats, or where shall we obtain one?

Lawrenceburgh Distillery.—We have heretofore mentioned that they have a large distillery in operation in Lawrenceburgh. The last Beacon gives the following account of 20 to 30 barrels of flour, grinds about 400 bushels of corn, and runs from 12 to 1500 gallons of whiskey per day. The proceeds for the last three months was about \$80,000, independent of the profits on the still slop; so we may safely estimate the yearly proceeds at \$300,000.

10-19-1838

"THE CANAL.—*en. Long, the Commissioner who has the White Water Canal in charge, has been promising our citizens that the water should be let in on the whole line from Brookville to this place, before the meeting of the next Legislature, or he would forfeit his claim to a re-election. We do not*

know how the work progresses in other places; but if we may be allowed to take this end of the line as a same, we shall despair of seeing a canal boat from Brookville short of this time next year. Why is it that there is nothing being done to the basin, the locks, and the waste weir at this place? Will Gen. Long explain! And will our friend of the Brookville American have the goodness to jog his memory on the subject! His re-election is at stake."

The foregoing is from the Lawrenceburgh Beacon of Oct. 6th. For Gen. Long we make no apology. We were of the opinion that he was rapidly progressing with the work under his charge. But prompt to the request of friend Gregg, (as we are to all the requisitions of the public upon our services) we called upon the General and informed him of the grievances of our friends at Lawrenceburgh, and that he must attend to their wishes or forfeit his claim to an election. He said the work on the Canal was progressing in a manner which he thought should be satisfactory to every one—That there were not more than a half-dozen contracts but what were nearly finished, and that he entertained little doubt but the Canal would be ready for water before the meeting of the Legislature. And we thought we saw something in the General's countenance, which intimated there were some people that all the patience, perseverance and power of mortals could not satisfy.

We understand that the contract for the basin and outlet at Lawrenceburgh has been entered into, and that it will be done as soon as the business on the Canal will require.—And as for waste weirs, don't fret friend Gregg, as we doubtless will have plenty in that line, and to spare, so soon as we have a good freshet in the White Water. And by the by, we are credibly informed that a prominent citizen of Lawrenceburgh has a contract on the line, and that it is more backward than any section between Brookville and Lawrenceburgh. Will the editor of the Beacon jog his memory?

For the re-election of Gen. Log, we hope there will be no necessity. The opinion seems to be universal that the number of the Board should be reduced. It is hardly probable that the next Legislature will persist in the unwise policy of continuing so large a Board, so detrimental to the interest of the State, as every member of the Board acts for the good of his own particular section —thus prosecuting the public works in many places where they can be of no benefit for years. Were there only three members of the Board (and that is sufficient to transact all the business,) the works of importance would be carried forward, and in places that portions would be of benefit so soon as finished.

We know the members of the present board have been guilty of things which are censurable, but have they done worse than others would have done? They have taken

THE WHITEWATER CANAL

as much as they pleased for their salary, and where have we any public officers who would not do the same, provided they knew if would be disregarded by a profligate Legislature. Each member of the board has stove to promote the interest of his own particular section of the State, sacrificing the State upon so selfish an altar, but where is the man the officer, who would not have done the same thing, when placed in the same circumstances. As for Gen. Long, we have heard of little complaint of his manner or prosecuting the work under his charge, saving his neglect of our Lawrenceburgh friends; and when the election comes on, if friend Gregg can present a candidate better qualified than the General, then we will take that matter into consideration.

11-2-1838

Canal Boats.—It will be seen by an advertisement that a boat yard has been established at Rochester in this county, and we are informed by one of the proprietors that they will have a packet boat finished in a few weeks. Gen. Long has also commenced a boat in this place and intends having it ready by the 20th of this month, when the water will be let into the canal from this place to Lawrenceburgh.

CENTRAL CANAL.—It is stated in thèndianapolis papers, that the water will be let in of the 10th inst., when they intend having a small jollification.

11-2-1838

CANAL BOATS.

The subscribers have established a Boat Yard, for building Canal Boats at Rochester, on the White Water Canal. Two of the company are regular ship carpenters of long experience, and will be engaged in the construction of boats in a few weeks. They solicit the patronage of the public. They have good lumber ready, and boats will be built, at reasonable notice. The business will be transacted under the stile of "T. Morse & Co." T. MORSE, U. KENDALL, S. COFFIN, B. G. CHILD. Oct 24.- 43-2w

11-23-1838

Damages.—The Board of Commissioners to assess the damages on the White Water Canal are still in session. They have decided several cases. If we are not mistaken, they have virtually decided that the Internal Improvements, and especially the White Water Canal, is of no benefit to the citizens. This, coming from a court organized under our laws, and solemnly sworn to the discharge of their duty, has almost made a "classifier," alias "Anti Improvement" man of us. If one man has a thousand acres of land, the canal passing through it, taking only 17 acres, and he is entitled to 500 dollars damages, we are of the opinion the Improvements are of little benefit to the State or individuals. And just such a case

has just been decided in the following list. We believe these decisions have been made contrary to the spirit and meaning of the Improvement Law, and contrary to the intention of those who framed the law. The justice or legality of these decisions we do not call in question. We have been furnished with the following list of damages allowed:

W. West,	30
Thos Shank,	400
Fin. Cooley,	275
John Cooley,	520
Allen Backhouse,	500
Wm McClure,	150
J. Bennet,	500
	2375

When our citizens are really damaged by the Canal, we are for paying them. But when the bounty of the State has constructed the valuable improvement through their large bodies of land, enriching them by enhancing the value of their real estate and surplus produce, and then paying 100 dollars per acre damages for land not worth more than from 5 to 20, is what we do not approve. Much of this, too, has to be paid out of the pockets of those who are not benefited by the canal. For instance, is it right or just that those citizens living in the eastern part of Springfield or White Water townships, who are not benefited by the Canal, but who pay their proportion for its construction, should still be drained to pay \$100 per acre for the stony bluffs along White-Water, which would be neither tillable or inhabitable were it not for their commercial advantages? Those who are acquainted with the stony banks, and spouty bluffs, between this place and Trenton, will be better able to decide this matter. We have not yet heard the amount of damages allowed on the lower end of the line, but should they be in proportion to this end, the damages between Brookville and Lawrenceburgh will be over 100,000 dollars. We know of several farms, which are worth 50 or 100 per cent more than they were before the location of the canal, the owners of which are now claiming damages to more than the value of the farms in 1834. There are some farms, mill seats, &c., which are damaged, and should be paid; and we believe some in the foregoing list were entitled to something. We shall continue the list of damages as we receive them.

11-30-1838

Since our last we have received the following additional awards of damages on our canal;

Warren Tebbs,	800
Gen. Arnold,	900
Mr. Moore,	40
Mr. Prudent	25
G. H. Dunn, (for mill seats)	1500

MR. CLARKSON:

Sir —In the last 8 or 10 days the question, "When will the water be let into the

Canal" has been frequently asked, and for the information of the public on this subject, you will please to say in your paper that the canal is now ready for the water, from this place to Lawrenceburgh, with the exception of two or three backward sections, which will be ready in two or three days, by the time the water can reach them from the place of its introduction.

It is now being introduced at the feeder 4 miles below this place, and also at that near Harrison, and the canal is ready for its introduction from the East fork feeder; but as the reach to be supplied by it is short and may be filled at any time in one or two days, it will not be let in until some work, not absolutely necessary to be done previous thereto, is completed.

I am informed by the Engineer that a greater amount of work has been done the past season than during the year before, and great praise is due to the contractors, and others concerned, for the faithful performance and energetic prosecution of the work, and so soon as the ice may be out in the spring they, and the citizens generally of the White Water Valley, will begin to reap the rich reward to which their enterprise justly entitles them. A SYSTEM MAN.

1839

1-18-1839

CANAL LETTING.—Proposals will be received at the office of the Cincinnati and White Water Canal Company, in the city of Cincinnati, until the 20th day of February next, for the construction of Forty sections of the Canal, averaging half a mile in length. The work to be let includes all the sections not heretofore put under contract, and two sections that were abandoned. The work consists mostly of common plain excavations and embankments, several stone arch culverts of 10 to 12 feet span, road bridges, &c. &c. Sections No 26 and 31 embrace each in culvert of 30 feet span, with heavy embankments running across them.

CINCINNATI CANAL MARKET.—Jan. 14.

	no sales.
Flour, (from store,)	6,50a6.75,
Do do wagons,	6,12 ½
Corn do do	62½
Whisky,	40a41 cts.
Oats,	50
Corn Meal,	62½
Wheat, per bushed,	1.15
Butter, (kegs)	16½ a 20

Cincinnati Gaz.

WABASH AND ERIE CANAL LANDS.

We have just time to state, that information was received in town yesterday that the Attorney-General of the United States has decided that Indiana has a right to the lands claimed by her to complete the Wabash and Erie canal to Terre Haute. These lands amount to near 300,000 acres, and were nearly all selected last summer, by the

THE WHITEWATER CANAL

authority of the Governor. *Tri-Weekly Journal*. Jan. 14

2-25-1839

CINCINNATI CANAL MARKET.

February 25, 1839.

Flour, (from boats,) 6,00 dull.

Whisky, 39a40cts.brisk.

Wheat, per bushel, 1,18 ¼

Corn Meal, 62 ½

Corn, 56 ¼

Butter, (kegs) 12 ½ a 16

Oats, 50

Cincinnati Gazette.

5-4-1839

ELECTION OF DIRECTORS of the Richmond and Brookville Canal Company.

THE subscribers of the stock in said Canal, will please take notice that an election will be held at Richmond [Wayne co] on the 15th day of June next, at 10 o'clock, a.m. for the purpose of electing seven directors for said company. Any subscriber voting as proxy will be required to present written authority from the person for whom he may claim to vote. By order of the commissioners. W. M. LEEDS, Secretary. Fifth mo. 4th, 1839 20--

6-14-1839

Board of Public Works.—We understand that this Board met at Indianapolis on the 4th inst., and organized by electing Noah Noble President, and Jas. Morrison Secretary. WE have not heard what they done, but presume they were *classifying, modifying and nullifying* as contemplated by the last Legislature. It is probable we shall know some of the particulars by our next. Rumor says that the process of *annulling and expunging* has been applied to all the contracts on the White Water Canal above Somersett.

The Canal Open.—The first Canal Boat, (Ben. Franklin) owned by Long & Westfield arrived at this place from Lawrenceburgh, by the Canal, on Saturday evening last, on which occasion our citizens let loose to their feelings of joy, by the firing of cannons and other manifestations of pleasure, calculated to be experienced at the successful completion of such an enterprise. On Sunday morning the Packet Boat Little Western, owned by Wood & Woods, arrived, and in the afternoon of the same day, the packet Niagara, owned by Squire Watts, arrived. They all left this place on Monday morning, for Lawrenceburgh, We believe it is the intention of some of these boats to ply regularly between this and Lawrenceburgh, but none of the owners have as yet authorized us to state the days or hours of their arrivals or departures.

East Fork Canal.—This election for Directors of the Canal takes place to-morrow at Richmond, after which some definite action we presume, will be taken upon the subject of the immediate commencement of the work. At this time there is from 14 to 25,000 dol-

lars paid in and lying idle. It should either be expended on the line, or invested in some manner profitable.

6-7-1839

The Court for assessing damages on the White Water Canal are now in session. They were to the place on Wednesday and Thursday and decided a few cases. From here they will proceed down the line.

They have allowed the following damages.

Mr. Stringer, \$330

Widow Lefforge, 450

Wm Tyner, 1,000

7-5-1839

Lawrenceburgh and the Canal.—We believe the Canal is now ready for business, and if Lawrenceburgh ever expects to be any thing or do any thing, this is a very important era to engage in it. They now have every chance to commence and hereafter control the trade and commerce of the White-Water Valley., if they will engage in the work with that spirit and liberality necessary to succeed in such an enterprise. They are not merely on an equal footing, but they have the advantage over any other point, and all the trade, wealth, and business, of this rich valley will be thrown into their lap, if they do not drive it from them by a contracted and niggardly policy. It will be at least two years before the Cincinnati branch of the White-Water Canal will be finished, during which time, Lawrenceburgh can, if they choose, so engross the business as to retain it after the branch is finished to Cincinnati. But a picayune business will never affect it. They must enter the work with that liberality, which only can sustain large commercial transactions. There are already several departments of trade and Mechanical Arts carried on there, of which we are deficient, but who can supply the demand here as well as we can be supplied from Cincinnati. Let them enlarge their business, and sell as cheap as at Cincinnati, and let themselves be known through the advertising columns of the Public Newspapers. Let their wholesale Merchants, wholesale Grocers, Apothecaries, Commission Merchants, Foundries, Lumber Merchants, Stone Cutters, dealers in Iron, Salt, Coal, &c. &c. advertise liberally in the Lawrenceburgh Beacon, with a request at the bottom of the advertisements for them to be published in some other papers in the White-Water Valley, (if they please the Indiana American) and we will insure them that their business will increase and prosper, and their town flourish, and soon become a large commercial city. They can sell every article as cheap as they can at Cincinnati, and it will cost less to transport it here, and if we can be as well accommodated in our own State, it is better for us and for the State to have our business done there. Gentlemen, try the experiment.

7-5-1839

The new and splendid Packet Boat, NATIVE, built at Rochester in this county by Mr. Coffin, arrived at this place by the canal on Wednesday last. It is represented as being one of the first order of passenger boats, and we hope the proprietor will have ample evidence, from the number of his passengers, that his exertions are approved by the public.

7-26-1839

Loans — Our Fund Commissioners have lately made another loan of \$600,000 for internal improvement purposes. This makes one million that has been loaned this year. The loan in the spring, which was stated in the papers to have been 4,000,000, was only 400,000.

Lawrenceburgh.—By reference to our advertising columns it will be seen that Lawrenceburgh is making an effort to do business in the right way. We hope our citizens will give them a fair trial.

We have before us a list of the claimants for damages on the White Water Canal, amounting to about 75 in number, the aggregate of whose claims exceeds \$131,000.—This is a formidable list, to be sure, and yet, with a few exceptions, these men who are claiming such high damages from the state, would not have the White Water Canal struck out of existence for twice the amount of their respective claims. It has been told to us, with how much of truth we are unable to say, that AMOS LANE has been encouraging these applications for damages, and promising additional facilities to the claimants should (he) be elected to the Legislature. If this is the case, the people should know it, for every dollar, which is paid in shape of damages, comes out of their pockets, without their having any return for it. To us it looks most unreasonable, that men should set up a claim for damages whose property has been enhanced in value more than fifty per cent by the canal. Yet such is the nature of man—his cupidity is unlimited. —*Law. Bea.*

8-2-1839

The Canal.—For the information of the Contractors on the White Water Canal, we publish the following letter, directed to Mr. Sivall, by Mr. Willey. We give the information as we find it. It is probable, the State has a sufficiency of funds to continue operations, but we question whether any more than the \$270,000 can be appropriated until after the meeting of the next Legislature. But the information contained in the letter of Mr. Willey will, we think, be a sufficient guarantee for our contractors to prosecute _____.

CONNERSVILLE, IA. July 29th, 1839

Dr. Sirs, I am happy to say to you that since I saw you I have had an interview with Gov. Noble, in relation to the prosecution of the White Water Canal. Mr. Noble informs me

THE WHITEWATER CANAL

that there has been a loan offered recently for \$600,000 and instructed me to say for the contractors (on the work) and all other concerned, that there will be a sufficiency of funds to carry on all operations on the White Water Canal during the present season. Such are the facts stated to me by Mr. Noble; and you are at liberty to make what use of them you see proper. Yours in haste, JOHN WILLEY

Mr. SIVALL Brookville, Ia.

8-16-1839

Cincinnati Branch of the White Water Canal.

It is stated in the Cincinnati papers that the work has been suspended on the Cincinnati branch of the White Water Canal, for the want of funds to continue operations. It appears to us that all the energies of the giant city of the West should be brought into requisition to complete this important improvement. It is of more importance to Cincinnati than any other improvement, which they have under taken. We hope the delay in this work will be short.

8-16-1839

MR. EDITOR:--I canalled, once, in a county where contractors were considered equally reputable with the best citizens of the county, not so here. They are put about _____ between the least honorable and most debased beings of the county -- which stigma, I fear we shall entail upon our children, and perhaps not to be eradicated for an age to come. Perhaps there is one remedial change than can be effected: for the contractors to honor the canal. For in consequence of the debased situation of the canal, it cannot impart any honor to the contractors. W.J. Payne, Canal Cont.

8-16-1839 Commission Warehouse.

J. WOODS would respectfully inform the citizens of Brookville and vicinity that he has opened a commission warehouse in Brookville on Main & Burgess street, near the Basin of the Canal, where he will receive and forward all goods that may be entrusted to his care. He will also receive and sell on commission all goods, wares and merchandise, left with him for that purpose. Also, he has an interest in the Canal Boat Little Western, which plies regularly from this place to Lawrenceburgh. Any persons wanting passage or freighting can apply at said warehouse, or to George Wood at Lawrenceburgh, or on board.-All business entrusted to him will be promptly attended to. Brookville, Aug. 15th, 1839 33-1y

12-20-1839

We see it stated in some of the papers in the State that there is (are) no boats running on the W. W. Canal, for the want of business to do. This is not true. On the contrary, we are assured by the owners that boats are running regularly and that business is rapidly increasing. There has been

no interruption to the navigation on the canal except two or three days during the freeze about the close of November.

1840

1-10-1840

NOTICE TO CONTRACTORS AND OTHERS. -- Under the authority of two Joint Resolutions of the General Assembly, the Acting Commissioners have been directed by the Board "to settle up in full with the Contractors of this state," at the contract prices, including the ten per cent hitherto retained, (except upon jobs and suspended) and for other items, for which they may be entitled to pay, (such as charge for extra work) and to give to said Contractors certificates for the amount due, bearing an interest of six per cent. But as the certificates that will be given may be transferred, and as many of the Contractors are not citizens, and may wish to leave the State, for the security of laborers and all others who have claims upon them, the Commissioner is authorized to withhold, for their benefit, all the adjusted claims that may be presented for that purpose:

To carry into effect the provisions of the enactments alluded to, the Resident Engineer will prepare all the necessary estimates with as little delay as possible, and to hasten the final adjustment of all the claims contemplated, it will be well for the Contractors to prepare an abstract of the claims due to Laborers, Farmers, Merchants, and others, that they may wish to settle in the manner provided for by law.

As the State has had to pay large sums, on the finished portion of the works, for timber claims, not demanded in time to charge the Contractors before their jobs were completed, I request the Contractors to settle with the land holders for timber, stone, &c. purchased for their jobs, so as to embrace all such claims, and avail myself of the opportunity to say, for the future, that unless such claims are demanded before a final settlement with Contractors, so that they may be entered to their accounts, I will not feel under obligations to make provision for their payment until other wise directed by law.

To comply with the new duties imposed by the recent legislation alluded to, I will by myself, or agent, attend at the following places on the White Water Canal, on the days specified:--

At Connersville on Thursday, the 23d day of January, for that part of the line from Laurel, including Section 93 to Cambridge.

At Brookville, on Monday the 27th day of January, for the balance of the line below Section 93.

All persons holding claims on any of the Contractors which they wish retained, will present them, properly adjusted, at the place and times above mentioned.

N. NOBLE, Ac't Com's. Jan. 4, 1840

2-7-1840

CANAL CLAIMS

We are requested to say, that the drafts given for the claims of others upon the contractors have been left with Wm. M'Cleery, Esq. for delivery, including all the line from Laurel to this place, except sections 90, 78, and 79, which are left at the store of E. Tyner, in Metamora. The labor claims on Sopris & Co. will receive on section 72 about 5 cents to the dollar, and on 87 about 27 cents, which will not be distributed until the money is ready, the sum due to each being too small to justify issuing drafts.

2-14-1840

MAS. J. TEMPLETON, JOSEPH SANDS and CHARLES PERSEL, are candidates for the office of Constable of Brookville township, at the approaching April Election.

The bill for the relief of contractors has finally passed both branches of the Legislature. We understand that the Governor will neither approve nor veto the bill, but will retain it in his possession over five days, when it will become the law of the State without the Executive signature. The features of the bill are such that the Governor, it is said, cannot conscientiously approve it, but is willing the contractors and the people should have the benefits of the bill, as it is probable if the Governor should veto it, there would be no other bill passed.

We are pleased to learn that the tunnel of the Cincinnati Branch of the White Water canal is still progressing. The following from the Gazette will give the reader some idea of this stupendous work, and of its state of forwardness:

NORTH BEND TUNNEL --This work is becoming quite an interesting object of curiosity. The perforation under the hill is advanced horizontally 460 feet, and a semicircle arch of brick or 24 feet span, resting upon stone abutments, raised five and a half feet high, is completed 400 feet in length. The workmen are now engaged exactly under the summit of the ridge, being 120 feet below the surface of the ground, at the apex of the hill. It is only 500 feet further to the base of the hill on the opposite side; where the work already begun upon that side would be met, which would open a passage from one side of the hill to the other. Within the last thirty days, 100 feet in horizontal distance has been completed. At the same rate, in five months, a tunnel will be completed entirely through the hill. The material is of the most tenacious (diluvium.) Fossil wood of a species of cedar is found embedded in the clay, at the depth of 100 feet below the surface. Engineers from the east, who have visited it, pronounce it the most perfect tunnel in the

THE WHITEWATER CANAL

United States. It is well worth a visit to see it while in progress. The operations are carried on through the night as well as the day.

2-28-1840

Travel between Brookville and Cincinnati -

The White Water Canal is now open, and will not probably close again the present season. Arrangements have been made between the owners of the Canal Boats Native and Franklin, and the Steam Boat Indiana, to carry freight and passengers between Brookville and Cincinnati. A canal boat is to leave Brookville every evening at 6 o'clock, and arrive at Lawrenceburgh next morning in time to take passage of the steam boat Indiana at 7 ½ o'clock, from Cincinnati and arrive at Cincinnati at 10 o'clock. The steam boat remains at Cincinnati five hours (from 10 till 3) being the business house of the day, and long enough to transact ordinary business, when it leaves for Lawrenceburgh, where it arrives at 5 ½ o'clock P. M. A canal boat leaves Lawrenceburgh immediately for Brookville, where it will arrive early next morning -passengers thus being two nights and one day making the trip, and having the business hours of the day to transact business. The expenses of the trip would be, for passage to Cincinnati and back, including boarding, \$4.00,
Diner at Cincinnati - .50,
One day lost, (worth) \$1.00,
Whole expenses by boat \$5.50.

Now let us see what it costs by stage, allowing time to transact business at the city. The stage runs tri-weekly, and arrives at Cincinnati late in the evening and leaves early in the morning, consequently we are compelled to await the next stage, and will have to tarry in the city two days and three nights -and performing the trip in four days and three nights. Passage in the stage to Cincinnati and back, \$6.00
Dinner on the road going & returning, 75
Fare at an ordinary respectable house
At the city for 3 nights and 2 days, 5.00
Four days lost, (worth) 4.00
Whole expenses by stage, \$15.75
Difference in routes. \$10.25

Apart from the difference in cost, and the time, there is another consideration, which should induce our own citizens to prefer the boat line -part of the passage money goes to the State and will assist in lightening our burthen of taxes. Those too who have goods, can take their freight with them on the boat, and see that they are safe and delivered in time. We have the utmost confidence in the proprietor of the S. B. Indiana, Mr. James, and the proprietors of the Canal boats Native & Franklin, that they will do every thing to render passengers safe and comfortable and deliver freight with care and dispatch. Freight from Cincinnati to Brookville, merchandize, per cwt 25 cts, salt per bbl. 50 cts, flour per bbl. 37 ½ cts.

We hope to see this line patronized as it

deserves, and hope the proprietors will be punctual in their arrivals and departures.

4-3-1840

Cincinnati and White Water Canal -We understand that the Cincinnati and White Water canal company, are issuing checks or drafts, payable twelve months after date, with which they are paying off contractors, and with which they intend to finish that arm of the White Water canal the present summer. It is said arrangements have been made with the Cincinnati Banks, so that these evidences of debt are received in the banks of the city. They have our best wishes for their success.

Unanimous. -On the canal boat Native on Saturday last, (we heard a Van Buren man say) a vote was taken upon the subject of the Presidential election and it was unanimous in favor of Van Buren. The passengers were on deck, and just as the boat was passing under a bridge, it was requested of all who were favorable to Van Buren to stoop as they passed the bridge. This was the way the vote was unanimous - they were compelled to stoop or be knocked off the boat. -Such forced votes as these will do to trumpet the popularity of Van Buren.

4-13-1840

Cincinnati and White Water Canal - We understand that the Cincinnati and White Water canal company are issuing checks or drafts, payable twelve months after date, with which they are paying off contractors, and with which they intend to finish that arm of the White Water canal the present summer. It is said arrangements have been made with the Cincinnati Banks, so that these evidences of debt are received in the banks of that city. They have our best wishes for their success.

Unanimous -On the canal boat Native on Saturday last (we heard a Van Buren man say) a vote was taken upon the subject of the Presidential election and it was *unanimous* in favor of Van Buren. The passengers were on deck, and just as the boat was passing under a bridge, it was requested of all who were favorable to Van Buren to stoop as they passed the bridge. This was the way the vote was unanimous - they were compelled to stoop or be knocked off the boat. - Such forced votes as these will do to trumpet the popularity of Van Buren.

6-7-1840

RICHMOND AND BROOKVILLE CANAL.

The stockholders in the Richmond and Brookville Canal, are hereby notified that two dollars on each share, of the second installment now due will be required on the 1st day of 9th mo. (September) next. By order of the Board of Directors. Benjamin Fulghum, Sec.

6th mo. 20th, 1840 Richmond Palladium.

7-7-1840

RICHMOND AND BROOKVILLE CANAL - The stock holders in the Richmond and Brookville Canal are hereby notified that two dollars on each share of the second installment now due will be required on the 1st day of 9th mo. (September) next. By order of the Board of Directors. BENJAMIN FULGHUN, Sec. 6th mo. 20th, 1840 Richmond Palladium

8-14-1840

From the Cincinnati Daily Gazette

CINCINNATI & WHITE WATER CANAL This canal is now in rapid progress towards completion. Upwards of one thousand men and five hundred teams are engaged upon it. The works are in progress throughout its whole extent. The first six miles south of Harrison will be completed in six or eight weeks, and the water let in to consolidate the banks. The large aqueducts across the Miami river and the Dry Fork of White Water, are in a forward state. The materials are being prepared at Muddy creek, Rapid Run, and Mill creek for the construction of the large arches over these streams, the foundations of which were all laid last year. At Rapid Run the centers are up and the arch is commenced. At Mill creek they are now putting up the centering to lay the arch upon. The large culvert over Bold Face creek is completed.

The great tunnel at North Bend passing under the Hill, which separates the Ohio and Miami rivers, is now excavated 680 feet from the southern entrance on the Ohio into the bowels of the earth. The distance is less than 300 feet to be executed to meet the works advancing from the north side of the hill. The earth in which the tunnel is excavated continues to be of the same blue clay, highly favorable for the operation of tunneling. I am informed that the contractors expect to complete the connection from the two ends in three months. The bricks and other materials are nearly all prepared for the completion of the remaining portion of the arch.

We publish the following for the benefit of our readers:

LAND SALE -Public Land Sale of Wabash and Erie Canal Lands.

In accordance with the provisions of the law, the undersigned will offer for sale at Peru, Indiana, by public outcry, on Monday the fifth day of October next, about 700,000 ACRES OF CANAL LAND, situated in several reservations recently purchased of the Miami Indians, in the Wabash Valley between fort Wayne and Logansport; all of which land is within five miles of the Wabash and Erie Canal, now navigable for ninety miles, and which will be opened to Lake Erie probably in eighteen months. -The very favorable location of the lands, with the well known richness in soil of the Miami reserves offer inducements for investment and for settlement, greater than have usu-

THE WHITEWATER CANAL

ally been presented at large sales.

9-11-40

THE WHITEWATER CANAL CO.

The Whitewater Canal Co.'s post notes have almost entirely superceded every other description of small currency in the city and are surrounding counties. They were at first received with great reluctance and were only disposed of at an enormous discount. The late fines imposed upon either unchartered companies have had the effect of sending their issues in a great measure home; and as their place must be supplied by something, the public seem disposed to take the Whitewater notes into favor, and they now pass readily in small amounts though they are still purchased by the Brokers at a discount of from eight to ten per cent.

We have before us a statement of the condition of the Company, which shows that at the 1st of January 1840, there had been expended \$217,513, and to complete the canal a further amount of \$282,486 would be required. At that time there was due to the Company from the State, the city of Cincinnati, and individuals, the sum of \$232,186, leaving a balance of \$50,000 to be raised by individual subscriptions and further appropriations by the state.

They have issued since the first of January, about \$134,000 of notes payable at some future day of which about \$40,000 have been redeemed. Thus it appears that there is no doubt of the ultimate redemption of the notes, though they may not be at present available in large quantities.

CAUTION.—Look out for counterfeit five dollar notes on the Whitewater Canal Co.—The following is a copy:
5 No. 456, letter A, dated April 11th, 1840, payable to order of S. M. Howard & Co. endorsed H. Johnston & Co.; J F Foote, Sec. J Bonsall, President.

The vignette is a very good imitation — the signatures are so very bad that they would at once lead in a detection. The paper is poor and very flimsy. It may be well to state that the Company did not issue any fines until after this note is dated. Con. Rep.

12-4-40

Speculators are endeavoring to put down White Water Canal paper. They, or their friends it is probable, have to pay some installment on their subscription to the company, and wish to buy in the paper at half price to pay their stock with. Speculators are always the first to assist in getting such paper in circulation, and then the first to refuse to receive it at its face. We wish the public to watch such men. Have nothing to do with them. They are living upon your honest industry. We see no reason why White Water paper is not as good now as it was when these men were so anxious to give it credit.

12-18-1840

From the Cincinnati Daily Gazette CINCINNATI AND W. WATER CANAL.

The present working season having terminated, it is thought proper to present to the public a summary outline of the present condition and future prospects of this important work.

This work was commenced in the spring of 1838, under favorable auspices. The estimated cost of the work, (exclusive of damages for right of way) being \$450,000, was all subscribed. The city of Cincinnati having subscribed \$200,000, the State of Ohio \$150,000, and individuals \$100,000.

The work progressed favorably until the fall of 1830, when the difficulty of collecting our stock from the city, State, and individuals, was such as to cause a temporary suspension of the work.

In March last, the Board of Directors being aware of the very heavy damage that would result from further delay, and having confidence in their ultimate resources, resolved to recommence the work, if they could obtain a credit of twelve months on the same, by issuing their notes to whom they might be indebted. For the payment of these notes, when due, they pledged the State, City, and individual subscriptions the due to the amount of \$232,500. In addition to this, they pledged the Canal and all revenues arising therefrom, for their ultimate redemption.—Under this resolution the work progressed equal to the anticipation of the most sanguine, until we were arrested in our advance by the approach of winter.

The present condition of the work, as reported by the Engineer, is as follows:

“There are 21 sections, making 12 miles and 13 chains, actually completed; of this 5 miles and 21 chains from Harrison to the Dry Fork Aqueduct, are finished consecutively; the others at various intervals throughout the remaining distance. 13 sections, making 7 miles and 12 chains, are very nearly finished. The remaining 11 sections, making 5 miles and 65 chains, are in a very forward state, and can all be completed with ordinary exertions by the 1st of July next.”

The report of the Treasurer shows the following to be the financial condition of the Company.

LIABILITIES.

Noted in circulation \$149,123

ASSETS

Due from the state of Ohio \$75,000

Due from ind. Stockholder 52,850

\$127,856

Expended in Canal const. \$471,267

We have resolved not to continue to issue our notes, except to satisfy some small claims due for labor performed up to the present time.

We have also resolved to endeavor to provide additional means, by the opening of the ensuing spring, to prosecute the work to completion. Joseph Bonsall. Pres.

Sam'l E. Foote, J.S. Harrison, T. Walker,
S. S. L'Hommedieu, J. Burnet. Directors.

1841

2-9-1841

BROOKVILLE, Feb. 9th, 1841

RECEIVED this day by the Canal Boat “Native” upwards of Twenty Hundred pounds of Castings viz. Sugar Kettles, assorted sizes, Dutch Ovens, also lids for the same, Skillets and lids, assd. All of which will be sold low. J. WOODS

4-2-1841

CANAL MEETING.

A meeting will be held at Laurel on Thursday the 15th of April next, for the purpose of taking into consideration the propriety of finishing the Whitewater Canal, as far as Laurel. It is proposed to organize a Company, and prosecute the work under provisions contained on the 5th and 6th sections of an act of the last Legislature of Indiana, viz:

“Sec. 5th. It shall hereafter be lawful for any Individual or Corporation to take up and finish any unfinished portion of the public works in this State, which may constitute a continuous line, and where the same is finished, they shall be placed under the control and discretion of the Board of Internal Improvements.

“Sec. 6th. Where any Individual or Company, or Corporation, shall be desirous to prosecute any portion of the unfinished works, they shall be at liberty to proceed to let out such contracts on the same as they may desire, under the direction of the Board of Internal Improvements, and enjoy the use and profit of such, portion as they may finish: *Provided*, That the State shall have right to take such works so finished at any time after ten years, by paying the Individual or Companies the amount expended with six per cent interest.”

The work proposed to be finished will doubtless be of great utility, and should be completed without delay as the State has already paid our rising two hundred thousand dollars, leaving the balance necessary to complete the work, one hundred and thirteen thousand dollars. It is believed that the contractors will take one fourth of the stock, and if all the public spirited men of Decatur, Rush, Fayette, and Franklin counties would put to their helping hand, the work can be done without embarrassment to the country. It is believed that the work can be completed in the course of the present year. Stock holders can, if they choose have 3 or 4 years to pay for their stock. It is confidently believed that the work would not only be of public utility, but if finished, will pay a fair per cent on the investment. It is desirable that all who feel an interest in the work, should be present at the proposed meeting. The contractors, board of Improvements, and every body else, are invited to

THE WHITEWATER CANAL

attend, Editor's that feel friendly, will please give this a place in their columns. Laurel, Ind., March 17, 1841.

4-23-41

CANAL MEETING.

At a public meeting held in Laurel on the 15th day of April, 1841, to take into consideration the measures necessary to finish the White Water Canal from Brookville to Laurel, O.H. Neff, Esq. of Rush county was called to the Chair and A.S. Babbitt and B. G. Wells appointed Secretaries. The object of the meeting being stated by the Chair, the following resolution was unanimously adopted:

Resolved, That we *can* and *ought* to finish the Canal to this place.

On motion a committee of seven consisting of Messrs. Mount, Shoup, Edgarton, Pierce, Lockwood, Harvey, and Clendening were appointed to report to this meeting a suitable plan to organize a Company to finish the work, who after retiring a short time reported in favour of forming a Company under provisions of the act authorizing limited partnerships.

On motion those of the Contractors present were requested to inform the meeting what proportion of the amount of their contracts they would take in Stock in the Canal —after retiring to consult they reported by Mr. Kelly that they were willing to take one fourth of the amount in Stock.

It was then resolved that this meeting be a Committee of Vigilance to solicit subscriptions and report at the next meeting how much Stock can be taken upon the following conditions, to wit:

1st. How much Stock can be taken payable in for annual payments with interest. Stock secured by bond and security.

2nd. How much can be taken payable quarterly in two years, with bond and security.

On motion David Mount, James Conwell, Hezekiah Mount, Joel Wilcox, and O. H. Neff be appointed A Committee to draw up articles of association and report at next meeting.

Resolved, That the different Editors who feel friendly to the undertaking be requested to publish the above proceedings.

The meeting then adjourned to meet again at this place (Laurel) on Wednesday the 12th day of May next at 10 o'clock A. M. O.H. NEFF, Chairman, A.S. BABBITT, B.G. WELLS, Secretaries. Laurel, April 15, 1841.

4-30-1841

Notice to ALL. LOOKOUT FARMERS!!

THE Subscriber having a first rate new Canal Boat which he will have in soon, he will carry all kinds of freight for all who may favor him with their custom.

He will also buy all the WHEAT that may be delivered to him, one and a half miles below Brookville, on Tyner's farm, and pay

the highest market price at all times. He is now prepared for Wheat, and by 20th of May, he will buy all the CORN that may be delivered, and pay the highest price.

AGESILAUS ROCKAFELLER.

April 19, 1841. 17-tf

5-28-1841

CANAL MEETING.

At a public meeting held in Cambridge City, on Monday, May 3, 1841, for the purpose of taking into consideration the further completion of that portion of the unfinished work on the Whitewater Canal between Brookville and the National Road, on motion of James Reed, Nathan Johnson was called to the Chair, and A.F. Shortridge appointed Secretary. G.W. Whitman then addressed the Chair, informing the House at some considerable length of the object of the meeting.

On motion of James Reed, Resolved, That a committee of ten be appointed to correspond with the citizens of Brookville, Laurel, Connersville and Milton, and ascertain their views as to the propriety of facilitating operations on the Canal. The House then appointed the following gentlemen said committee:—F. Johnsonbaugh, John Huddleston, William Petty, James Will, J.H. Hill, C. B. M'Ray, G. W. Whitman, S.Y. Sharp, John Beard, Jr. and C. W. Ligget.

On motion of G.W. Whitman, Resolved, That the above committee be and they are hereby authorized to appoint a public meeting at some suitable time in Cambridge City, by giving general notice of the same.

On motion of G.W. Whitman, Resolved, That the proceedings of this meeting be published in the Wayne County Record, the Watchman, and Brookville American, and that the citizens on the line of said Canal be requested to hold meetings in order that there may be some definite understanding and concert of action in regard to taking stock in and commencing said work.

NATHAN JOHNSON, Ch'n.

A.F. SHORTRIDGE, Sec'y.

6-4-1841

Cincinnati and Whitewater Canal. Laborers & Stone Masons wanted.

THE work on this Canal is again resumed, and the Company have determined to complete it during the present year. All the necessary arrangements to secure its completion having been made, public notice is hereby given that several hundreds additional Laborers, and 20 to 30 good Stone Masons, will find employment and prompt pay in cash, by application to the several contractors along the line of the Canal. Apply to:

Messrs. Hamilton Ashby, at Dry Fork

" J & J Dickey, at Miami River, Muddy Creek and Rapid Run

" S & H Howard & Co., at the Tunnel at North Bend

" Joseph Cooper, at Millcreek near

the city.

D. LAPHAM, Engineer,
May 31, 1841 23-3w

6-11-1841

NOW READY!

THE Subscriber having finished his New Boat, he is now ready to purchase any amount of Wheat or Corn. Until he can make other arrangement he will occupy Gen. Long's Ware House near the Eastern Bridge, where Corn and Wheat can be delivered. The highest cast prices will be given.

He is now running his Boat and is ready to carry freight or passengers. His Boat is new, safe and convenient, for freight, or comfortable for passengers.

A. ROCKAFELLER. June 9, 1841 17-tf

9-17-1841

Third Veto!

The following, which we copy from the Cincinnati Gazette of the 15th inst. Is more disastrous news to this section of the country than half a dozen Bank Vetoes. Are all our hopes of prosperity to be blasted, and the people become slaves and menials!

THE CINCINNATI AND INDIANA "WHITE WATER CANAL"—It is a subject of some consequence to the public, and therefore we state, that we regret to learn that an injunction has been obtained from the Court of Common Pleas, of Hamilton county, by W. S. Hatch, Mr. Resor, Mr. Purnell, and others against the White Water Canal Company.—The operation of this injunction, is to suspend further proceedings upon this work, just as they were proceeding with the greatest spirit, when the work could be done at the lowest rate, and the canal was about being finished. The suspension of this work, when the time was so near at hand for the commencement of business upon it, and the city would be receiving some income from it, thus lessening our taxes, is much to be regretted. The opening of this canal for business, bringing at once, as it would have done, a large trade from Indiana into this city, is of great public interest to our merchants and mechanics, who would thus have opened to them a new channel of trade. The commencement of business on the canal would, at once, give an impetus to trade, and change the aspect of things in the part of the city, where the canal terminates. The suspension of the work not only throws out of employment a large number of working men, but puts off the receipt of the last installment from the State, which can only be claimed when the work is finished, and thus necessarily postpones the payment of any debts due by the company, by lock up their funds. As but few things could cause greater loss than to suspend the work on a canal in an unfinished state, and cause it to lay over for a winter's frost, we earnestly hope, that some arrangement will be made, whereby the company may progress with their operations, although we

THE WHITEWATER CANAL

fear that this injunction places matters in such a position as that it will be necessary entirely to stop.

9-17-1841

WHITE WATER CANAL. From the *Indiana Telegraph*.

The most of the persons incorporated as the White Water Valley Canal Company met in Connersville, Indiana on Wednesday the 16th inst.—JAS. CONVELL, was chosen President and HENRY GOODLANDER, Secretary. The Charter was adopted and all arrangements made for opening Books for the subscription of the capital stock, on the 4th of April next, in New Castle, Knightstown, Cambridge, Centerville, Milton, Connersville, Rushville, Laurel, Brookville and Lawrenceburg in the State.—The books will be continued open under charge of commissioners appointed for that purpose, at each of said places, during a period of ten days.

Stock may be subscribed to be paid for in real estate, situate in this state; in labor to be done on the canal and in money, The money subscriptions are required to be secured by bond and mortgage or other acceptable security, the first \$5 required to be paid on each share, in nine-equal annual installments, the first to be paid 1st of March 1844. The first \$5 required to be paid on each share of the capital stock, must be paid in money or by note due one day after date; and if upon the election of the directors the note be secured to their satisfaction, its payment may be postponed until the first March 1843 —by-laws, &C. were agreed upon which will be sent out, in due time, to each of the places named above, with the stock books by which details in reference to these matters may be fully seen.

The capital stock will be so well and amply secured, and that being backed by the work given to the company —work which has cost the State more than one million one hundred thousand dollars in cash —is contemplated that abundant credit will be given to such certificates of stock taken, as to enable the company to finish the canal to the national road in two or three years. To complete this, and in all things agreeably to the charter, it is estimated will not cost the company more than \$100,000 —the capital stock required before the canal is transferred to the company. The stock then must be unusually valuable —and the canal can be finished as early as anticipated —the benefits of themselves will nearly pay out _____ (this part is missing).

9-17-1841

Wheat has advanced in this place since our last to 90 cents. And although the quantity offered for sale is great, yet the demand is greater than the supply. Our merchants are doing a fair business in the purchase of produce, and the sale of goods, salt, iron, &c. Business appears lively —the

boats on the Canal are scarcely able to carry off all the produce ready for shipment. Our people begin to see the advantages and convenience of a Canal.

1842

2-25-1842

Have just been favored by Mr. TYNER, By way of the *Indiana Journal* of the 22nd which contains the acts for the relief of the _____ yet taken ten per cent

White Water Canal.— We had written a short exhortation upon the subject of this canal, but having since received the *Connersville Telegraph* containing an article upon the subject, we have laid ours aside, and now call the attention of our readers to that subject. It is useless for us to add another word, in addition to the article, further than to state that in all matters looking to the completion of this great work whether by the state or companies, it has our hearty approbation.

The East Fork Canal company are to hold a meeting at Richmond on to-morrow evening to organize under the provisions of the Internal Improvement law of the last legislature. They wish to avail themselves of the privilege of issuing scrip notes.

2-25-1842

WHITE WATER VALLEY CANAL. STOCK NOTICE.

Notice is hereby given that on Monday the 4th day of April next, Books will be opened, and kept open ten days for receiving subscriptions of Capital Stock of "THE WHITE WATER VALLEY CANAL COMPANY" under the provisions of the Charter, at the following places in the State of Indiana, to-wit:

New Castle, Knightstown, Cambridge, Centerville, Milton, Connersville, Rushville, Laurel, Brookville, and Lawrenceburg. Stock may be taken to be paid for in Real Estate, in Labor to be done of the Canal, and in money, the last to be paid in ten equal annual installments. At each point, together with a Stock Book, may be seen the charter and the general Rules as to the taking and payments of Stock; all of which will be shown by the Commissioners appointed at each place to superintend the receipts of Subscriptions; who will give any explanations needed. All desirous of making safe and profitable investments, will do well to look to this matter in season.

JAMES CONWELL Pres't. of the Company, H. GOODLANDER, Secretary, Connersville, Ind. Feb. 16, 1842

3-25-1842

Cincinnati and White Water Canal.—We congratulate our readers upon the prospect of speedy completion of this important work to this portion of country. There is now very little doubt but we will have the advantage of this canal for our fall trade. The following announcement we find in the last

Cincinnati Gazette. The Dickey's are vigilant and persevering men:

Cincinnati and White Water Canal.—We understand that the Board of Directors have issued a contract with Messrs. John and Joseph Dickey, by which they agree to complete the whole of the canal remaining unfinished, and to have the Canal ready for the reception of water by the first day of August next. — Cin. Gaz.

4-13-1842

NEW LINE. CINCINNATI & BROOKVILLE CANAL AND STEAM BOAT LINE.

The owners of the Canal Boats PATRIOT and CHESAPEAKE inform their friends and the public in general that they have formed a REGULAR LINE with the STEAM BOAT WM. PENN, and are prepared to take freight and passengers on the most reasonable terms. One of the above Canal Boats will leave Brookville on Mondays, Tuesdays, Thursdays, and Fridays at 9 o'clock A.M. and arrive at Lawrenceburg on the evening of the same day, where the freight and passengers for Cincinnati can take the Steam Boat next morning at 7 ½ o'clock, and arrive at Cincinnati 10 ½. The Steamer Wm. Penn will leave Cincinnati regularly on Tuesdays, Wednesdays, Fridays, and Saturdays at 3 o'clock P. M. and arrive at Lawrenceburg at 6 P.M. where the above named Canal Boats, Patriot and Chesapeake, will be in readiness to receive on board all passengers and freight, and arrive at Brookville at 7 next morning.

The undersigned are determined to do all in their power to please all who favor them with their custom, and their prices shall be in accordance with the hard times.

WARE HOUSE. We have a New Large Warehouse immediately at the head of the Canal at Brookville, where all freight designed for the above line will be stored free of charge.

THOMAS. H. BARWISE, Owner of Chesapeake., A ROCKAFELLER, Owner of Patriot. April 1842.

6-10-1842

W. W. CANAL.—From what we learn from Connersville, there is now but little doubt remaining, but the work will be commenced on the White W. Canal between this place and Cambridge this summer or fall. The stock is now so nearly all taken that it renders the matter almost certain. Some 15,000 or 25,000 remaining unsubscribed on Saturday last, and we have heard of several subscribing small sums since.

6-24-1842

White Water Canal.—The *Connersville Telegraph* of the 17th contains the following intelligence: "We are happy to inform our readers that at a meeting held in this place this day it was ascertained that there had been a considerable amount over and above the estimate required to complete this work

THE WHITEWATER CANAL

taken.—That part of the canal now finished, will soon be placed in the hands of the company, and the unfinished portion of the work soon put under way for completion."

Official notice is also given in the Telegraph that there will be an election held at Connersville, on the 19th day of July for the election of nine directors for said company, agreeably to the charter.

7-1-1842

CANAL CELEBRATION.

There is to be a great Canal Celebration at Cambridge city of the 28th of July, to which all the friends of Internal improvements are invited. A committee of arrangements of 40 persons has been appointed to make the necessary arrangements. The committee was very liberally distributed among the various towns interested in the White Water Canal.—Geo.G. Shoup, of Laurel and Gen. Long of Brookville are two of the committee. Ample preparations will be made for an old fashioned Barbecue (sic), and great and distinguished men will be invited to attend, such as the Hon. Henry Clay of the United States, R.M. Johnston of Kentucky, Hon. Thomas Ewing, Gov. Corwin, Gov. Shannon, and others of Ohio. Gov. Bigger, Noble, Wallace, Howard, O.H. Smith, &c. &c. of Indiana. To those who have not visited that beautiful and fertile portion of Indiana and the White Water Valley, it will be a pleasant trip. We trust many of us will be found there.

7-13-1842

WOOL CARDING And Cloth Dressing.

THE undersigned having entered into the above business at Harrison, on the White Water Canal, would inform the public generally, that his machinery is entirely new, and of the best quality that ever was in the west. He has employed an attentive and experienced journeyman to attend the machines, and with their united exertions hopes to merit a liberal share of public patronage. The machines will be in full operation by the first of June. W. BRIGGS
May 18, 1842 21-2m

1843

6-23-1843

"*Whitewater Canal.*—We learn that the recent freshet has done considerable damage to this work, on the finished portion, between Brookville and Lawrenceburg, and that navigation has ceased in consequence. What is the nature of the injury, or the amount that will probably be required to make the repairs, we are not informed."—*New Castle Courier.*

We have no doubt the Editor of Courier will take great pleasure in informing his readers, that not one word of the above paragraph is true. No damage has been done to the Canal between this place and Lawrenceburgh, and that there has been no

interruption to the navigation.

6-30-1843

WHITEWATER CANAL.

The Courier of the 17th published at New Castle, Ia., says the adverse cloud which for a while rested upon the Whitewater Valley Canal Company, and threatened the entire cessation of work on the Canal, has been dissipated *entirely*, and that a more prosperous and encouraging state of affairs exist at present than at any time since the commencement of the work. It is now confidently believed that fourteen miles between Brookville and Laurel, will be finished, and the water let in, by the 1st of August. If this be so, (and the Courier says it is placed beyond contingency,) the Canal will be completed to Cambridge City probably within two, and certainly within three, years.

This is good news —goodnews for us, but better, if possible, for our Indiana friends.—They little dream of the advantages which the Canal will confer upon them. Let the people of Henry County, for example, remember the condition of either of our valleys, before Ohio had begun her public works, and contrast the thin population, and the small business done, with the dense settlements, which have occupied, and the large and thriving business which has been done in them, *since* the completion of these works, and they may form some idea of the benefits in store for them. Fertile lands are valuable any where in this State of Indiana. But a good outlet for produce, such as a canal furnishes, will quadruple their value by cheapening the conveyance of produce to market, and filling them with hardy settlers, and giving to those who live upon them a motive as well as opportunities to turn their industry and capital to good account. It is, therefore, a matter of congratulation, that all difficulties on the Whitewater are over come, and that the worst job on the line is so near completion: for from the commencement of the work, the great struggle, as the Courier will observe, has been in getting the Canal to Laurel. Once there and all is safe.—*Cin. Gaz.*

9-8-1843

THE CANAL.—Wanderstand that the Canal will be finished from the boundary ridge, (two miles above this place) to Laurel, some time next week; and it has been stated to use that the water will be let in at Laurel, on next Monday week. The section at Boundary Ridge, it is said, will not be done short of four or five weeks.

We also understand that the Tunnel of the Cincinnati and White Water Canal will be done in three or four weeks. We may, therefore, confidently anticipate that in six weeks the canal will be navigable from Laurel to Cincinnati.

9-8-1843

BROOKVILLE —ITS IMPROVEMENTS.

Notwithstanding locofocoism with its blighting and withering influence, which has touched with a mildew blight all parts of our beautiful country, and has weighed like an incubus upon the energies and enterprise of the citizens of Brookville, yet the business and prospects of our town were never more flattering. At no period within the last twenty years have more improvements been in progress, or more business done upon actual capital. Many of our citizens, we acknowledge, have been laboring under heavy pecuniary responsibilities, but none have bankrupted, and few have been compelled to close business. By economy, industry, perseverance, and forbearance with each other, we are emerging from our difficulties, establishing ourselves on more firm foundations —increasing our business upon sure capital,—improving our residences, enlarging our business houses, &c. Things about us begin to smile with an air of prosperity and happiness. We propose to notice some of the improvements of the present season:

No. 1. *Geo. W. Kimble* has erected, and will have in operation in about one month, a large and splendid flouring mill —four stories high, with four run of stone. Mr. Kimble is one of those enterprising men who are a benefit to any town in which they may locate; and he is entitled to much credit for this beneficial improvement to our town and country. From the workmen he has employed, and the materials he has obtained for his mill, we have assurances that everything will be done in the best style, and that the people may expect justice done them.

No. 2. *Messers. Coffins*, who have for some years had a large ware house at the basin of our Canal, have this year built an extensive addition to it, which new addition we also understand, will probably be changed into a flouring mill when water power can be obtained from the West Fork Canal.

No. 3. *N. D. Gallion* has built a fine brick store house. When merchants are making *permanent* improvements for business, it is a guarantee that they intend doing business on such liberal terms as to retain a *permanent* custom. Transitory merchants who set down among us, *shave* us for a few years, until the *shave* themselves out of business, and then remove to some other place to follow the same business.

9-15-1843

Lawrenceburgh—We have been glad to notice on a recent visit to our neighboring city, that she is rapidly regaining her former prosperity, which had been somewhat paralyzed by the difficulties that have beset nearly all kinds of business. The tinkering of the currency, and the consequent expansion of bank paper, and crazy schemes of internal improvement have doubtless afforded our neighbors a full share of the general dis-

THE WHITEWATER CANAL

truss; but the storm has about blown over, and a more healthy tone of business is growing up in every direction.

The opening of the White Water Canal has afforded a vast amount of water power to Lawrenceburgh, which is one of the chief causes of their present prosperity. There are already two Flour Mills, with fourteen run of stones, in operation, besides a Wool Carding Machine and a Saw Mill. An Oil Mill and another Flour Mill are now erecting, to be completed before winter; and there is yet unoccupied water power sufficient to run twenty pair of burr millstones.

About half a mile back of the town, stands the extensive distillery of Wm. E. Crafts & Co. This is worked by steam power, and we should judge it is one of the largest in the Union. It consumes 900 bushels of corn daily, producing upwards of 3800 gallons of Whiskey. The latter is sold chiefly in this city. — Connected with it is a Flour Mill which grinds 500 bushels of Wheat, and turns out 100 barrels of Flour, daily; and attached to the works is about the largest hog-sty in the West certainly, where, fattened on the slops of the Distillery, between three and four thousand monstrously large, quiet hogs can be seen in all the repose induced by the most redundant obesity.

With the advantages of the Whitewater Canal, and excellent roads, Lawrenceburgh has become the focus of a most fertile district of back county; and the quantity of Wheat, Corn, Flaxseed, Hay, &c., sold at that point is immense. In winter, we are told, it is no uncommon sight in their streets to witness daily 250 Wagons filled with agricultural produce. The amount of Wheat sold there from August, 1842 to August, 1843, is estimated at 140,000 bushels, and of Flaxseed 16,000. There is more pressed Hay sent forward from that place by flat boats, than from any other single county in the West. The exuberance of the bottoms extending back from Lawrenceburgh as far up in face as Dayton, is proverbial, where can be seen forty square miles of cornfield, all in a single enclosure.

The number of Hogs killed in Lawrenceburgh last winter exceeds 22,000.

The above branches of business naturally create a great demand for Cooper's Ware—and the number of adults employed in the manufacture of Hogsheads, Barrels, and Kegs is between 90 and 100, besides apprentices. The amount of work turned out by them last year exceeded \$13,000, much of which is sent to this city daily, as will be seen by our table of Imports. The new Oil and Flour mills now erecting, will give increased activity to this business *Cincinnati Gaz.*

10-13-1843
CANAL.—The *Indiana Courier* is slightly mistaken as to the completion of the Canal to Laurel, and the arrival of boats at that

place. There is yet some work to be done, and it will be several weeks before boats can pass from Brookville to Laurel.

WHITE WATER CANAL.—The work on the portion of the canal between Brookville and Laurel, we are gratified to learn, is fully completed and the water is being let in up to the latter place, and boats will in a few days arrive there.

We regret to learn, however, that the Commissioners employed to go East to negotiate a loan for the Company have returned without accomplishing their object, an account of the general distrust of all Western securities in the Eastern money market. The failure to effect the loan will have no other effect than merely to delay the completion of the Canal for a year or so. The only object of the Directors in working to effect a loan was that they might finish the work sooner than originally intended. — *Newcastle Courier*

10-27-43

The Canal from this place to Laurel is filling with water as rapidly as is safe. The water is within a mile or two of the place, and the probability is it will be ready for navigation in a week. It will be seen that the Canal Boat Patriot will make a trip on to-morrow week, accompanied by the Brookville Band. It is possible some other boats will also go up to Laurel about that time.

We understand there are to be meetings of the stockholders of the White Water Valley Canal, at various places along the line on to-morrow, for the purpose of pushing forward more rapidly, by some means, the work on the line. An effort is to be made to have the stock paid in 5 years, instead of 10 as first provided.

1844

2-9-1844

WHITE WATER VALLEY CANAL

The annual election for directors of this company was held at Connersville on Monday last, and resulted as follows; the nine highest being elected:

Miles Murphy,	3175	John Crum,	2011
Dan Sinks,	2859	C. B. Jackson,	2768
M. Helm,	2967	D. Hawkins,	2292
Sol. Claypool,	2038	David Mount,	3178
J. Dobinspike,	2829	A. Dale,	1957
John Clifford,	1030	G. Frybarger,	1731
A. B. Conwell,	1642	J. A. Wilson,	1566
Joel Hiatt,	1528	L. Lackey,	1383
Henry Reunan,	1280	John Powell,	1206

The new board met on the next day and re-elected all the old officers of the board.

3-1-1844

Lawrenceburgh, attend to your interests! — The tolls upon the White Water Valley Canal from Laurel to Lawrenceburgh have lately been raised about 30 per cent, whilst the

tolls on the Cincinnati Branch have been reduced about that amount. To us, this is a matter of little or no concern, but it may be a fact which has escaped the notice of our Lawrenceburgh friends. If they cannot see the effect of this regulation they are rather dumb.

For instance, the tolls on a ton of hay from Harrison to Lawrenceburgh, 13 miles, is 26 cents, and from Harrison to Cincinnati, 28 miles, 22 cents, and other things in proportion. Consequently, under the present arrangement, every thing to be shipped south, or to any market, will go by Cincinnati.

3-8-1844

WHITE WATER VALLEY CANAL.

The last Lawrenceburgh Beacon contains two articles upon the subject of the tolls upon this Canal. The editor complains of the arrangements that have been made to injure the business of Lawrenceburgh. Of this part of his articles we have nothing to say. But he concludes by inquiring "how long will the PEOPLE suffer such impositions?" Does not Major Dunn know the *people* have nothing to do with the canal? Does he not know the canal belongs to an incorporated company of stockholders —that it was dug with their money, and that they have a right to do as they please with it? Probably when you penned those article, Major, you were not advised that the canal belonged to one Dan. Hankins, one of the richest, wisest, and greatest men, this side of Hardinsburgh, *in his own estimation!* And if you were advised of this fact, were you not in dread of the vials of wrath that he sluices upon the heads of those who do not fall down and worship him! Had you only witnessed the final destruction and overthrow of the devoted little village of Brookville a few days since, by this man "who swells himself to bursting high, to utter bulky words of admiration vast," you certainly would not had the temerity to have asked so impudent a question, "How long will the *people* suffer such impositions?"

The fact is, Major Dunn, the canal does not belong to the people, but so long as we can enjoy some of its benefits, it is not prudent or wise to excite the multitude against it. How soon could an excited mob deprive the company and the people of all its benefits. It is better for us to endeavor to conciliate the directory by reason and kindness, than to be necessary to its destruction by exciting the idle and vicious. We have a deep interest in it and it is better for us to suffer many impositions than to be deprived of it altogether.

You are also, Major, sadly mistaken in supposing the "*Indiana American*" to be the organ of this company. We were crossed from their books many long months since, and so particular and vindictive has been the directors of the company, that *special*

THE WHITEWATER CANAL

instructions were given to the collector in this place not to have his blanks printed here, and their official advertisements have been diverted from us. — Of this we do not complain. The canal does not belong to us, and we have no right to claim their jobs. They can continue to manifest their bitterness by their special instructions against us, and we shall continue to advocate our interests and that of the people, by sustaining and defending the canal. We shall always frown upon any attempt to excite the rabble against it.

There is no doubt but the present tolls upon the canal are too high, driving much of the business from the canal, and making the freights upon it burthensome and almost intolerable, but they must be judges of these matters and not us. They are the representatives of the stockholders, and their interests must be consulted before any others. And although the *people's money* and not the stockholders, built that portion of the Canal we are interested in, yet we have transferred it over to them, and to their *tender mercies* must we look for a wise and judicious management of the concern.

3-15-1844

White Water Canal Money. — What is the cause of the attack on this currency by the last Connersville Telegraph? That paper contends that the money is only worth 50 cents to the \$1.00, thereby injuring its credit at home and abroad. That paper as well as other papers along the line should do every thing they could to sustain the issues of this company, instead crying it down. Is the editor opposed to the money or the canal? We cannot think that some of the large stockholders have employed him to put it down, in order that they may buy it up for a song, or to pay their installments. We have a better opinion of Mr. Stewart than this and must attribute it to some higher motive.

We give more than 50 cents for it in payment for debts, and if there are any persons along the line who have taken it at its face for produce or labor, we will take it at its face for new advance subscriptions to the American. We wish to assist in sustaining the money, and not denounce it as not being worth 50 cents to the dollar.

3-22-1844

The Canal. —The Connersville Telegraph has screwed itself into a considerable rage because we dared defend the Canal from the attacks of the Lawrenceburgh Beacon and the Connersville Telegraph. But we care about as little for what the Telegraph editor can say, as we do for those puppies from whom he borrows his stereotyped slang. From men unqualified to say any thing else, we look for such stuff. We only expect decent and respectable language from decent and respectable men. Probably the editor has already noticed that we regard and re-

gret only the abuse of those only who are enlisted to credit; and hence has taken considerable liberty. For a dime we will grant to some editors the full, free, and unmolested privilege of abusing us, and consider we should be making money at that.

?-?-1844

The Cincinnati and White Water Canal. —A Breach occurred in the Canal on Thursday evening the 4th inst. It is supposed it will require two weeks to repair the breach. The Gazette of the 8th contains the following information: The breach in the Canal, as we stated on Saturday, occurred some two miles below the city. The navigation, however will not be suspended in consequence of it. A dam has been erected just beyond the breach, and the company have reduced the tolls so as to cover the drayage from that point to the city, to remedy, as fast as it can, all inconvenience arising from this accident.

5-3-1844

Lawrenceburgh — We paid a flying visit to Lawrenceburgh on last week. The Circuit court was in session at that place. This is the first session since the removal of the County seat. The old Court House has been repaired, so that it is convenient, neat, and comfortable. The citizens of that place look in good humor at the favorable settlement of the vexed County seat question.

Business in Lawrenceburgh appears lively and active notwithstanding the folly of the Canal Company drives all the business to Cincinnati. The Canal and Basin at that place looks as silent and desolate as a mill pond. There is only one boat that navigates that branch of the Canal, making about one trip a week, annoying considerably the ducks and geese.

Joseph G. Marshall was to address the citizens of Lawrenceburgh on Tuesday the 25th inst. We regret that we are unable to remain and hear him. The Whigs of that county are aroused and active, and are confident of carrying the County at the August Election. The locos divided and distracted. They have too many office seekers among them.

?-?-1844

BROOKVILLE, IND.

The country looks beautiful, and we wonder that our friends do not visit if oftener.

Among the many rides we know of none more inviting than the ride to Brookville. — This village is delightfully situated among the hills, in a fine farming region, and has advantages which few of our towns possess. The water power there, (and in the whole valley,) is extensive, and we were glad to find it beginning to be improved. Flour mills, two small cotton factories, and a fine paper mill belonging to Speer and Stevens, are already in existence. We exam-

ined the latter with some care. Col. Speer manufactures most excellent paper, and keeps up with the improvements of the day. Another fact, too, we may mention as showing the worth of the man—on conversing with his workmen we found that all of them had been with him for years, and one for 23! They had prospered with him and were content.

The canal connects us directly with Brookville. But the turnpike road from Harrison ought to be finished, also, to that place. The road in wet weather is bad. Why cannot the farmers along the line stir up this subject and act upon it? A good road always pays for itself. The increase value of the land through which it passes, more than meets the outlay. Let our friends see if they cannot accomplish this object —there is every facility for it — every motive to do it.

There is great improvement going on in the country all around us. Farm houses, the fences around them, show that our people are looking more and more to comfort — and to the possession of those means which add so much to human happiness. And morality, too —the advancement is as great. Not a tavern in Brookville sells liquor; we saw no dram shop in the place! The people have voted that these things shall not be and they are not!

Brookville, as well as the country above it, and Hamilton of our State, as well as Dayton are destined to be great manufacturing places. They will be the Lowells of the West. Like Boston, then Cincinnati should make herself the center of a web of turnpikes and rail-roads, embracing this region, so as turn our water power and resources to the best account — and the more we hasten on this result the more this whole region will thrive. Farmer, merchant, manufacturer, day-laborer — and mechanic— are all interested, and should exact their influence, whenever they can, to help along the good work. Push out the turnpikes whenever you have the ability to do it! *Cin. Gazette.*

6-22-1844

The Line of Packets — have been compelled to tie up until the cholera shall abate. At present very few will risk a visit to the city, even on urgent business.

8-2-1844

SALE OF TOWN LOTS IN ROCHESTER.

THERE will be offered for sale on Friday, the 6th day of September, 1844, a large number of lots in the town of Rochester, Franklin county, IA. These lots are in a new plat, just laid out between the old part of the town and the Canal lock, and are situated on both sides of the Canal.

The town of Rochester is on the White Water Canal, 34 miles from Cincinnati, and six miles from Brookville. The Canal and State Road run through the center of the town. —There is a lock in the town of ten

THE WHITEWATER CANAL

feet fall, making excellent water power. It is in the midst of a fine country, and the hills adjacent abound in excellent timber for boat building, a large business of which is carried on in the town. The town is healthy, and pleasantly situated. There is a large flouring mill in operation in the town.

TERMS. —One third in hand, one third in a year, and the other third in two years. A title bond will be given for a deed when the last payment is made. The title to the property is good.

JOHN GREEN, Proprietor.
July 27th, 1844 32-lm

1845

1-31-1845

WHITE WATER VALLEY CANAL.

The editors of the *Cincinnati Gazette*, being more lucky than ourselves, have received the regular annual report of the White Water Canal Company, showing the transaction, means and prospects of the company for the year ending Jan 1st, 1845. We gather our information from the *Gazette*.

Of the \$432,000 subscribed, \$264,076 have been paid up, leaving unpaid only \$170,823. A majority of the stockholders, in order to complete the work here pledged themselves, to pay two installments annually, on the 1st of March and Sept., thus completing their payments in 5 instead of 10 years allowed by the terms of subscription. This effort and the arrangements made with Mr. Vallette, it is supposed will secure the completion of the canal the coming year.

The revenues of the company consist of	
Tolls collected at Harrison	\$1493 75
" " Lawrenceburgh	135 90
" " Brookville	2759 10
Water rents collected	<u>1354 57</u>
	\$5743 32
Deduct expenses collection	396 00
Do repairs	<u>4773 51</u>
	5159 00

The repairs last year were thorough, and made expensive by a wet weather and freshets. Notwithstanding the delay of this the tolls show an increased sum received from that source for the last over that of any previous 6 months of \$529.

Up to the present time the cost of construction amounts to 250,258 77 dollars, of which 61,275 01 has been worked out in payment of Stock.

The means of the company consisting, besides the canal, of real estate, debts due, is estimated at 512,758 dollars, and their liabilities of all kinds exclusive of the Stock to 80,758 dollars.

During the last year, the entire work between Connersville and Cambridge city has been let to Henry Vallette of Cincinnati, who is to receive pay in the bonds of the company at their face, payable in 10 years with interest at the rate of 7 per cent per

annum, payable half yearly. This contract dispels all doubt of the early completion of the whole work to Cambridge.

The claims for damages for land, &c. on the line above Brookville remain unpaid, but the whole liability on such accounts, are not expected to exceed 30,000 dollars.

4-25-1845

White Water Canal. —This great public improvement is fast progressing to completion, and will, in all human probability, be ready for the fall business to Cambridge City. Already, we understand, has the water been let in at the feeder dam just below Connersville. The aqueducts over Garrison Creek, just above Laurel, and over White Water, just below Laurel, will be the last work finished this side of Connersville. But we understand these aqueducts will be done in the month of May. —We presume therefore that we shall have an opportunity of celebrating the 4th of July with our friends at Connersville by water communication.

With all the burthen and disgrace of a public debt which we *do not* pay, we would this day rather it should continue to hang over us, than to dispense with the benefits and convenience of the White Water Canal. We were always in favor of Internal Improvements, and with all its unpopularity, we are not yet ashamed, or regret that we advocated such a scheme. We know it was wrongly managed, but this we never countenanced or apologized for. The White Water Canal will make this valley the richest and most independent part of the world. We are yet young, but with our fertile soil rich and inexhaustible —there is nothing to impede our progress to wealth the most unbounded. When we recollect what has been done in this valley within the last twenty-five years, who can estimate the wealth, splendor and magnificence, which shall beautify and adorn this valley in fifty years. Mansions and palaces will displace the neat cottage farm house, as they have driven out the log cabin, and our posterity will enjoy in luxury our labors and the toil of our fathers. But without some great public improvement, we could never have been any thing but hewers of wood and market peddlers. Few of us fully appreciate the benefits of Canals, Railroads and Turnpikes.

5-11-1845

From the Cincinnati Chronicle
White Water Valley and Canal.

The valley drained by the White Water river is composed of the counties of Franklin, Fayette, Wayne and Union with parts of Henry, Randolph and Dearborn. The White Water Canal will be the highway for the surplus productions of these and many of the adjoining counties. No definite or satisfactory idea can be formed of the sum total these productions will reach a few years hence, when the immense water power and

the whole of this fertile valley is improved and settled. It is supposed that a finer body of land for agricultural and mechanical purpose is not to be found in the State. It compares favorably with the Miami valleys.

The principal towns lying along the line of the Canal, and their distance from Cincinnati are:

Towns	Miles
Cleves, Ohio	15
Harrison, Ohio and Indiana	21
Rochester, Indiana	37
Brookville, " "	43
Metamora, " "	53
Laurel, " "	59
Connersville, " "	71
Cambridge, " "	81

The towns of Somerset and Laurel have been the terminus of the Canal for some months past. The water was let in the Canal one mile below Connersville of the 14th inst. The aqueduct, crossing the White Water at Laurel, has as many men employed on it as can work to advantage, and is rapidly progressing. The line thence to the National Road will be ready two or three weeks later.

Of the towns named above Connersville is the principal, containing nearly double the population of any other. It is handsomely situated, on a second bottom, with the river on the east, and the canal on the west. It has very good facilities for trade. The canal lies along the foot of a rise of hills of easy ascent, averaging perhaps 80 feet in height, a view from which commands an extensive area of improved farms beyond the town and east of the river. —The water power near and in the town, along the whole course of the river, is abundant, even during the driest summers; and several mills lie along its course, in sight of the town, all of which are actively employed. Several new brick and frame houses for dwellings and stores, are built and building. The town is well planned, the streets graded, and altogether it has a healthy, agreeable appearance. The canal locks about this town are numerous and well built.

The blue limestone quarried on Williams creek, five miles from Connersville, is found to be superior for building purposes, to any other known. It is exceedingly hard, without veins, and may be quarried to any desirable size. It sustains a good polish, and may be sawed for sills and lintels; and would doubtless become an article of trade upon the completion of the canal.

The distance from Cambridge city to Harrison is 56 miles. The rise is overcome by 32 locks, averaging 10 feet each. The dams and locks, together with the other work on this canal, will compare favorably with any in the country.

For the benefit of whom it may concern, and particularly for those who may wish to change their location, we understand there is to be a large public sale of in-lots at Connersville on the 19th, and one at Laurel on the 28th and 29th of May. April 20, 1845 W

THE WHITEWATER CANAL

5-16-1845
THE CANAL.

It is understood that boats will go to Connersville on the 20th inst. We bespeak a few slices of roast beef at Mallory's, in Connersville, on that occasion. — We expect to see the citizens of Connersville in a good humor, at the consummation of a work for which they have so nobly, patriotically, and successfully struggled. Connersville has done much in this matter, and narrow-minded and selfish is he who will not now award them the necessary credit. They went into the work in the right spirit, with their talent and their MONEY, and now their hope is in full fruition. We feel none of the spirit of envy or jealousy at the future prospect. If they have done more than we could have done — and we believe they have — if their prospects are brighter for the future — which we sometimes doubt — we will rejoice with them. For the last three years we have admired the zeal, the energy, and the perseverance with which they have applied themselves to this work, although we have at the same time objected to the overbearing spirit manifested by some of their citizens. But Connersville has many liberal and public spirited men — more probably than any town of the same population in the State — who are an honor and an ornament to our valley. May health, harmony, wealth and prosperity attend them a thousand years.

5-21-1845
FARMER'S & MERCHANTS DAILY LINE
From Cincinnati to Laurel

The proprietors of the new and substantial Canal Boats *Native, Swallow, Belle of West, and Cambridge City*, have united for the purpose of running a Daily Line for freight and passengers from CINCINNATI to LAUREL. One of the Boats will leave Cincinnati every day (Sundays excepted) at 7 o'clock, P.M., and arrive at Laurel, next day a 6 o'clock, P.M. — Leave Laurel every day (Sundays excepted) at 7 o'clock, A.M., and arrive at Cincinnati at 5 o'clock next morning.

The proprietors having incurred a heavy expense to render the facilities for freight and travel as expeditious as possible, hope from their experience and attention to business, to merit a share of public approbation.

N. B. We have a Ware house at Brookville, and at Laurel, where all freight for the line will be received and forwarded with care and dispatch:

Our charges will be moderate.
S. COFFIN, Proprietor of *Native*
T. H. BARWISE, Pro. Of *Swallow*,
G. G. SMITH, Pro. of *Bell of West*
G. COLLINS, Pro. of *Cambridge City*.

April 18th, 1845 3-w.

6-27-1845
Miami Canal Packet Line from Cincinnati to Toledo

Messrs. Doyle & Dickey, the enterprising proprietors of the present packet line in Cincinnati, are prepared to take advantage of the opening of navigation to the Lake to put on a line of packets to Toledo. The following is a list of packets and Captains:

Banner,	Capt. Benj. Ayres
Fashion,	" Jno. R. Smith
Ocean,	" A. M. Scott
Tempest,	" S. B. Snediker
Neptune,	" M. Van Horne

Laurel.

Two of these packets, the Banner and Laurel, are fine new boats, the others have been thoroughly repaired and refitted, and will be found both comfortable and convenient for travelers — and every effort will be made to render the line worthy of patronage.

The "Banner," Capt. Ayres, will leave Dayton for Cincinnati on Monday, the 23rd; on Tuesday morning she will depart from that place for Toledo. This will be the commencement of the regular operations of the line. A packet will therefore, six days out of seven, leave Cincinnati for Toledo, and from that port for Cincinnati as soon as the first trip is accomplished. In the course of a few weeks the Laurel will be ready, and then a daily line will be run.

The distance from Cincinnati to Toledo 247 miles — The boats will travel it in 64 hours the fare through will be \$9.

On the upward trip, the packets will arrive at Dayton at the usual hour. It is not known at what time they will reach here on their way down — *Dayton durnal*

7-11-1845
Metamora

This town, located seven miles above Brookville on the Canal, should not escape our notice. It was laid out by Mount & Holland about eight years since. There is a little circumstance connected with the naming of this town, which may be forgotten unless put upon record. The proprietors not agreeing upon a name, it was proposed that each of the proprietors should write his choice name upon a slip of paper and put it in a hat. There was also present at the time a lady from Brookville, and it was agreed that she should also have the privilege of putting a name in the hat. Judge Mount selected "Farmington," Wm. Holland selected "Mount Holland," thus perpetuating the name of both of the proprietors. The lady from Brookville just having read an interesting novel in which a distinguished Indian heroine is introduced by the name of "Metamora." She therefore selected this name. The lot was shaken, and *Metamora* drawn out.

We visited Metamora on Saturday last. It is one of the most active business little towns that we have visited for some years. Mess. Gordens have just erected a new flouring mill there with four run of stones — this infamous order, we hope some of the new carding machine, and new store

house which they have stocked with new Goods. Jons. Banes is erecting and will have in operation in a few weeks a large Cotton factory. He is also building several fine dwelling houses. E. Tyner & Co. have a fine store, and a large ware house, and are doing a large business in the purchase of produce and in storage, commission, &c. We noticed that a large portion of the Goods for Rushville is stored at Metamora. Wm. Rubottom has also a fine stock of goods, and deals heavily in purchase of produce. He has a very convenient ware house for the transaction of business. He is also preparing to build a flouring Mill. Mr. Blacklidge has a fine saw mill. The various branches of mechanic are pretty well represented at the town. Two physicians and one lawyer. — It is destined to become a rich, healthy, prosperous manufacturing town. They don't speculate on borrowed capital there.

9-12-1845
Great Freshet. LAWRENCEBURG, Sept. 6.

On Wednesday last we had a tremendous rain in this place and north of this, which caused the small stream to rise suddenly to an unusual height, whereby considerable damage was done to property along the creek banks. The platform was washed from the new bridge across Tanner's creek at New town, and the creek was consequently impassible for a day — A considerable quantity of lumber was washed down the stream — between this place and Aurora the beach was covered with cantings which had probably been washed by the freshet from some lumber yard.

The canal embankment was broken away in six places by the flood of waters which fell on Wednesday night. In one place the break is large and the damage will be considerable. The accident has had the effect of stopping some of the mills, which were propelled by water power from the canal. The mill and distillery of Mr. Comegys, however, is provided with a steam engine so that in case of a failure of water the machinery can be propelled by steam power.

The break in the canal will be repaired in a few days. — *Register*.

9-12-1845
Canal Money. — Rumor says that the White Water Valley Canal Company have passed an order refusing to take, after the first day of October next, their own money now in circulation, in payment of tolls or water rents. When we shall become fully advised of their action in this matter and the nature of the order they have passed we shall give our readers our opinion of the measure. And if we should call things by their right names — and give some the private influences, which have operated to bring about more fiery will stand gently in the traces

THE WHITEWATER CANAL

under the lash.

The Canal. — By an article on our first page it will be seen that there are several breaks in the Lawrenceburgh branch of the White Water Canal. It is supposed that they will be repaired in three or four weeks.

9-19-1845

The Canal Company.

In another column will be found a clear and lucid exposition of the doings, intentions and position on the White Water Canal Company, by S. W. Parker, Esq. It will be read with profit by our readers.

When we first received the news of the offer, refusing to take their own circulation for tolls, our feelings partook somewhat of that of the people generally. And had we at the time indulged in remarks in accordance with our feelings, we should have said some hard things. But in view of the great and lasting blessings this company had conferred upon the people of this valley — begad and carried on amid pecuniary gloom and despondency, it is well to be slow in our reprehension. And notwithstanding we doubt the propriety, or even the honesty of the "order," yet we are willing to let the matter go to the people without comment.

There are strong and abiding prejudices in the public mind against corporations and companies, and we have found ourselves sometimes indulging toward them uncharitable thoughts. We are old enough to know better, and do better, but there is so much of *human nature* about us, that we have to be watchful.

The position, which Mr. Parker supposes the Company will take relative to the Canal Bridges, is reasonable. Let the question be fairly tested by the Courts of the country, and if against the citizens of the county, we shall use all our influence to have them abide the laws peaceably and quietly. It will be a grievous burden upon us, and many of the bridges will necessarily have to be abandoned, and the roads changed. But we hope ever to find the citizens of this county obedient to the law, of their country.

The people generally, boatmen, and shippers of produce, have no right to complain of the "order." It is only the holders of the money — those who have received it in good faith as contractors and laborers. Of the amount in their hands, Mr. Parker is better able to judge than ourselves.

11-14-1845

Business of the White Water Canal. —

As the American is now pretty generally patronized in every town on the White Water Canal, we have made arrangements for giving weekly an account of the Imports and Exports by the Canal, of the towns of Cambridge, Milton, Connersville, Laurel, Metamora, and Brookville. We wish it known that we obtain our statistics for these reports from the highest authority, and can be relied on as strictly correct. — Potatoes, bu. 6

We shall allow no partiality in our feeling to withhold from or add to the business of any of the towns. We intend the American shall be relied on as correct authority in these matters.

We also request the citizens of the several towns named, to give us occasionally an account of the progress of improvements in their towns. People abroad look to the American to find accounts of the business of this valley; and we intend they shall find it — correct and impartial. We have contemporaries along the valley whom we wish not to disparage, but they have not the room in their paper — they have not the experience to collect, nor have they the general circulation of demand so extensive commercial statistics.

It would be advantageous to our neighboring towns, if they would also send us weekly accounts of the prices of produce, and such other remarks as they may deem advantageous and interesting. Such things, business men will find will increase the business of the town in which they reside. In this way they can make our paper more interesting, and at the same time call public attention to their business.

11-21-1845

White Water Canal

The following is an account of the imports and exports from the towns of Cambridge, Milton, Connersville, Laurel, Metamora and Brookville for the week ending Thursday morning (yesterday) the 20th inst. Produce loaded or merchandize delivered along the line is added to the business of the town which is nearest the place where it is received or delivered.

<u>Cambridge</u>	<i>Exports</i>	<i>Imports</i>
Wheat, bus.	2,888	
Flour, bbls.	209	
Flax seed, bu.	30	
Butter, lbs.	280	
Tallow, do	380	
Merchandize, lbs		50,431
Salt, bbls		600
Iron & Castings, lbs.		9,209
Whiskey, bbls.		9
Sundries, lbs.		6,488
Leather, do.		1,096
<u>Milton</u>		
Wheat, bu.	958	
Merchandize, lbs.		1,382
Slat, bbls.		50
<u>Connersville</u>		
Wheat, bu.	1,506	
Cider, bbls.	13	
Sundries, lbs.	6,010	8,993
Merchandize, do.		8,189
Salt, bbls.		150
Iron & Castings, lbs.		450
Lumber, feet		700

<u>Laurel</u>		
Wheat, bu.	1,804	
Salt, bbls.		88
Oats, bu.		78
— Potatoes, bu.		6

Sundries, lbs.	400	
Flax seed, bu.	13	
Lumber, feet	15,000	
Merchandize, lbs.		11,059
Salt, bbls.		217
Whiskey, bbls.		8
Sundries, lbs.		6,055
<u>Metamora</u>		
Wheat, bu.	2,841	
Flour, bbls.	25	
Sundries, lbs.	7,400	
Cotton yarn, lbs.	1,900	
Sassafras Bark, lbs.	400	
Merchandize, lbs.		9,000
Salt, bbls.		75
Iron & Castings, lbs.		3,742
<u>Brookville</u>		
Wheat, bu	285	
Oats, bu	150	
Pumps	100	
Pickles, lbs.	300	
Cider, bbls.	1	
Sundries, bbls.	3,933	2,469
Cotton Yarn, do.	1,377	
Butter, do.	300	
Paper, lbs.	1,646	
Lumber, feet	15,000	
Merchandize, lbs.		2,196
Iron & Castings		5,478
Moveables, lbs.		1,800
Empty bbls.	200	
Stone coal, bu.	100	

12-4-1845

White Water Valley Canal
SECRETARY'S OFFICE, W. W. C. CO. CON-
NERSVILLE, 4th N v., 1845

THIS certified that at a meeting of "The White Water Valley Canal Company" this day, the following was

ORDERED: That from and after the first day of March, 1845, no discount shall be allowed on advance payments of debts due the Company.

Witness my hand L. B. Tupper, Secty.
Nov 4th, 1845 46-4w

2-6-1846

White Water Valley Canal Company

The *Connersville Telegraph* gives the Report of this Company, which contains facts of some interest to this region. The contract of Mr. Vallette was completed, as our readers know, in July last. The navigation if now open to Cambridge City on the National road, the point to which it was originally proposed to carry it.

The principal items on account with the Company stands thus — *Cin. Chron. Price Current.*

Capital stock,	\$432,000
Cost of Construction,	423,000
Real Estate for Sale	21,201

The water power of the Company is represented as very valuable; and as affording good reason to believe that it will be of vital importance to the interest of the Company.

The Tolls from the 1st of June to the 1st

THE WHITEWATER CANAL

of December, was as follows:--

At Harrison,	\$3,450 90
" Brookville,	2,537 18
" Connersville,	2,521 47
" Lawrenceburgh,	326 47
	<u>\$8,836 32</u>
Water Rents,	\$2,236 28
Total Receipts,	11,172 59
Expenses of Repairs, &C.	7,614 72
Of the Debts and Means, that account is:	
MEANS,	\$183,567 94
Liabilities,	176,974 44

This is exclusive of the Canal itself.

On this exhibit the Company passed the following orders:

Whereas, This is the day prescribed for the regular semi-annual declaration Dividends — and it being ascertained that the net profits of the Company, at this time, after making the usual reservation for contingencies, making a Dividend Fund equal to *Two and one Half per cent*.

But inasmuch as said Fund is mostly composed of the Paper of the Company, and the balance is indispensable — unless a loan be resorted to — for the purpose of paying the February installment of Interest on the debt of the Company, created for Construction: Therefore

Ordered, That payment of said Dividends be surrendered, until means arise from payments of Stock in Cash, equivalent thereto, but of which the same may and will be promptly paid.

Provided however, that any debtor of the Company, owning any such Dividends, may have the same passed to his Credit on account, at any time, on request — or upon the order of ay owner — the same may be applied to the liquidation of any of the company's dues — not consistent with prior orders.

1846

2-6-1846

The Canal Question — The individual, who we expected would reply to the article in our last paper upon the subject of Comegys' suit against the White Water Canal, declines discussing the question until after the decision of the case by the Court.

Terre Haute. — On the reception of the news at Terre Haute of the passage of the bill for the arrangement of the State Debt, and to finish the Wabash & Erie Canal to Evansville, the citizens of that place held a general jubilee of rejoicing — the town was illuminated, and addresses of congratulation were made by R. W. Thompson, Col. Lane of Vanderburgh, and Baker.

The State Debt. — So far as we have heard an expression from the people upon the subject of the bill for the arrangement of our State debt, there has been but one expression and opinion — all heartily approve of it, and believe the Representative and Senators did the best that could have been done under the circumstances. The people

feel the stain of bankruptcy and silent repudiation is already partially removed. We have evinced a disposition at least to make some efforts to redeem our plighted honor. We feel better.

There is expected to be some opposition to all great measures. To this there is some. We believe the editors of the following papers opposed the Butler bill, to wit: Cambridge Reveille, Richmond Palladium, and Vincennes Gazette. We do not doubt their honesty in their opposition. Their grounds of opposition satisfy themselves, whilst it might not satisfy others.

4-10-1846

White Water Canal *For the Indiana American*.

C.F. CLARKSON Esq: --I presume that you and your readers are not aware that a new power is gradually developing itself in the State, and affecting in perform(ance) the double functions of a Legislature and a Court of Justice. It is a thing or primary importance that the people should know by whom they are governed; it is equally important that they should be advertised of the laws which it is the good pleasure of their rulers to establish.

The new power to which I advert, is the White Water Valley Canal Company. That body, after having received from the State as a free gift, a public work upon which the people had expended over a million of dollars, arrogates to itself great credit for the public spirit and energy with which is has pushed the extension of the work to its northern terminus. This public spirit has, to be sure, been made illustrious by some very brilliant financial operations. The wretched laborer who has toiled under a burning sun, and exposed himself freely to the diseases of the country, for the *nominal* pay of from 8\$ to 10\$ per month, has read the evidences of this public spirit upon the engraved paper of this Company, and has no doubt felt duly thankful for that energy which has paid him his pittance in promises whose value has fluctuated between the extremes of from 40 to 80 cents upon the dollar. The public has seen and duly appreciated the evidences of this self denying public spirit, this patriotic energy. They have yet to learn perhaps, that the company is extending its protection over them and providing them with new laws, and erecting for their convenience new tribunals of justice. The charter by which this Company has its existence, provides, that in the cases where individuals are injured in their real property by the making of the Canal, they shall call upon the Company for indemnity by the means of an arbitration. The claimant is to appoint one arbitrator, the Company another, these two a third; who, after inquiring into the matter, are to report their award in writing to the Secretary of the Company, whose duty it is to record the proceedings of the arbitrators, and in cases of appeal is

to be recognized as a Justice of the Peace for the purpose of certifying to the appellate Court a transcript of their proceedings.

Persons the least informed upon legal topics have heard that such a distinction is made in the law as renders some actions *local* others *transitory*; and that among the former are such actions as are founded upon injuries to real property. The effect of this distinction is to make all local actions triable in the county where the injury is done, for the obvious reason, that wherever an injury is done to property local and permanent, there is the most convenient place of calling together and examining the witnesses who are best acquainted with the nature and extent of the injury.

This reasonable and useful rule seems very objectionable to the company. They have endeavored to evade its operation on the ground that their charter constitutes their Secretary a Justice of the Peace for the purpose of preparing transcripts on appeal; and that inasmuch as that officer resides in Connersville, he must certify appeal papers to the Fayette Circuit Court, no matter upon what part of the line the injury may have been done. Accordingly in several cases from this County, appeals have been sent, at the company's instance, to the Fayette Circuit Court. But that court knew too well the rule of law governing such cases, and dismissed the appeals; a proceeding afterwards sustained by the judgment and opinion of our Supreme Court, to which tribunal the Company carried the question.

Not content with the termination of this sagacious attempt, the company has passed an order which is quite equivalent to an amendment of its charter, and which, as I conceive, goes the full length of making a new law for the people of this valley and of constitution a new court to enforce that law. This order provides that in all cases when damages are claimed against the company, the arbitrators selected, after they may have personally inspected the property said to be damaged, *shall meet* at the office of the Secretary of the Company, and upon being duly sworn (by the Secretary?) *shall proceed, at said place*, to hear any testimony that may be advanced by the parties and argument of counsel, and then make out and return to the Secretary their award.

Here then the opinion of the Supreme Court is fully reversed, and notwithstanding the local nature of such actions, the parties, their counsel and their train of witnesses, must assemble at the bar of this company, and perform their functions under the eye of the Secretary. Throughout the whole line of the canal from Cambridge City to Lawrenceburgh this law extends; the Legislature of the State, the Circuit and Supreme Courts dwindle into insignificance before a company which dictates and administers the law of the land. Whether, in cases when parties are pliant enough to regard this or-

THE WHITEWATER CANAL

der in council, the company will contend that the question of jurisdiction is waived, and that appeals must go from trials in Fayette County, to the Fayette Circuit Court, remains to be disclosed. In the mean time we hope our new law makers will provide for the publication of their enactments, that the public may be guilty of no violation of the company's code. Your obt. Servt.
J. D. HOWLAND. April 4th, 1846

4-17-1846
Brookville Belle.

THIS Packet boat, having been nicely fitted up for the accommodation of passengers, and also to take a small quantity of freight, will run regularly between Cincinnati and Brookville.

It will leave Brookville every Monday, Wednesday and Friday, at 4 o'clock P.M. and arrive at Cincinnati about eight the next morning.

It will leave Cincinnati every Tuesday, Thursday and Saturday at 4 o'clock P.M. and arrive at Brookville next morning by eight.

Farmers wishing to visit the city with a little freight will find this Boat very convenient as they are prepared to take freight as well as passengers, and can be relied on as departing regularly at the time specified.

Freight intended for this boat will be received by Mr. Moffet at Brookville Canal Basin and by T. J. Tyner Canal Basin Cincinnati.

Coal Yard —Coal always on hand and for sale by Mr. Moffet at the Ware House, Brookville Canal Basin.
ALONZO CRARY Master.
April 17, 1845 16-tf

5-8-1846
CANAL DEBTORS.

ALL persons indebted to the White Water Valley Canal Company, on any account whatever, will be expected to pay by the 1st of June next. The Company have determined to close up all cases of the kind, as soon as possible. Further indulgence need not be expected. Delinquent Stock will be sold as provided by the Charter, to the highest bidder —and any deficit not supplied by such sale, will be collected from the subscriber, at once by due course of law.
S. W. PARKER. Attorney for the Co.
April 13, 1846 19-3w

5-22-1846
DAILY PACKETS ON WHITE WATER CANAL.: ENTIRELY PASSENGER BOATS.

In order to meet the wants of the traveling public on this flourishing inlet, a *Line of Boats to run regularly between CINCINNATI & CAMBRIDGE CITY*, Has been fitted up in good order, and passengers who take this line may be certain that the proprietors will do everything for their comfort whilst under their charge. This is a DAILY LINE OF BOATS, Leaving each point regularly: --

Cincinnati at 2 o'clock P.M.; and Cambridge City immediately after the arrival of the Western Stage,

Fare through, each way, \$3.00, including meals.—They are at liberty to present the following references.

Nath'l M'Clure,
White Water Canal Basin, Cin.
Geo. Collins, Harrison, O.
Samuel Murphy, Brookville, Ind
N. Walker, Metamora, Ind.
I. Clements, Laurel, Ind.
Geo. Frybarger and J. Elliott, Connersville, Ind.
Aron Reasor, Cambridge City, Ind.
C.S. ANDREWS & Co., Proprietors.
May 18, 1846 21-tf.

7-10-1846

The Whitewater Canal, is drawn off below the city for repairs and cleaning out. There is a prospect of its doing a largely increased business this fall from its extension, and the heavy crops of Eastern Indiana. From the late Report made to Council, it appears the business of 1845 was more than double that of 1844 — the tolls being.

For the year ending	
Nov. 30, 1844,	\$1,330 83
Nov. 30, 1845	8 339 38
Nov. 30 th 1845 to May 30, 1846	4 309 81
"Water rents to date,"	1 577 32

The total cost of the Canal was \$838,108, 27 —of the stock of which, the city of Cincinnati owns \$400,000, and the State of Ohio, 150,000. — *Cin. Gas.*

8-21-1846

George W. Kimble vs. The Canal.

Since making up our editorial page the Circuit Court now in session in this place has decided the case of George W. Kimble vs. the White Water Valley Canal Company. In this case Mr. Kimble asked for an injunction against the Company, prohibiting them from using any more water in the Canal than was necessary for the navigation of the Canal. The case was ably argued by Messrs. Holland and Matson for plaintiff, and Messrs. Newman and Parker for defendant. The Court decided that in the charter of the Company, the State had granted full power and authority to the Company to take and use as much water as they pleased for either navigation or hydraulic purposes; but the State at the same time had granted ample remedy to persons injured by this diversion of water. The injunction was refused. The case will be taken to the Supreme Court.

8-21-1846

BROOKVILLE, INDIANA.
FRIDAY, AUG. 21, 1846.

The Canal. —The repairs of the Canal are nearly completed. The water is in at Laurel, all that part between Laurel and this place will be filled by Saturday. The water is already in between Harrison and Lawrence-

burgh, and we learn that in a few days navigation will be resumed on the entire route from Cambridge to Lawrenceburgh. The repairs on the Cincinnati branch will require some weeks yet. But in the meantime, by the assistance of the steam boats, we shall have a good communication to Cincinnati.

The Circuit Court has been in Session since last Monday week. The principal cases before the Court arise out of the construction and navigation of the Canal The Court will not be able to clear the docket during the Session. The case of *Comegys vs. the Canal Company* has been dismissed. The case of *Geo. W. Kimble vs the Canal Company*, for damages to his Mill property is before the Court at the present writing (Thursday) and eliciting considerable interest and talent, as a vital question to the profits of the Canal is involved. In this case the question is to be settled whether the company have the right to take any more water than is necessary for the navigation of the canal. Without the profits arising from water rents, canal stock will be of little value.

8-28-1846

High Waters — Canal.

Again has our valley been visited with a disastrous flood. Heavy showers fell in this region on Thursday night, Friday and Friday night of last week, which has proved disastrous to the fall business of this Valley.

The Canal has been empty for near two months and was just filled ready for the fall business —merchants, farmers and boatmen, were just looking on tiptoe for the profits of the opening trade. But their hopes were suddenly blasted on Friday evening last by the washing away of the Duck Creek aqueduct. This Creek rose in a few minutes to some feet higher than ever known before, undermined the abutments and the adjoining embankments. The whole valley of Duck Creek is said to have suffered serious damage. Haystacks, rails, corn, buildings &c. were swept away.

We learn that the company have already a large force at work there, and all that energy can do will be put forth to repair the breach. Our opinion is that it cannot be done short of two months. We hope it may be done sooner. The destruction of this aqueduct puts an end, almost, to business for two months: Mills, Factories, &c. are all stopped.

The aqueduct across Big Cedar Creek six miles below Brookville, also gave way on Friday last. But this can be repaired we learn in two weeks, when navigation will be opened to Lawrenceburgh.

The damage will cost the company at least, we think, \$8,000 or \$10,000 —the loss of tolls \$15,000, and the total loss to the mills, boatmen, merchants and farmers, would be poorly paid with \$100,000.

Our townsmen, Messrs. Spear and Stevens sustained considerable loss in washing

THE WHITEWATER CANAL

away the embankments, timber and lumber, where they were rebuilding the dam which was taken away last spring.

We understand that a canal boat owned by James White of this place was drifted over the dam and stove to pieces.

Most of the Canal boats on the line are above the Duck Creek aqueduct.

8-28-1846

Wheat. — We wish it understood that the Brookville merchants always pay the highest market price for wheat. The aqueduct across Big Cedar will soon be repaired, when we shall have access to the market.

9-4-1846

The Canal. —The aqueduct across Big Cedar will be repaired in a few days, when we will have open navigation to the river. Our merchants are ready to purchase Wheat at the highest cash prices, or to sell groceries and dry goods at the lowest prices.

Later and Better. — We have just received the following letter from Mr. Moore, by which it will be seen arrangements are making to facilitate the business of our valley. We are truly rejoiced to hear it. There must necessarily be a large amount of wagoning from Metamora to Brookville, and it would be well for teamsters to prepare themselves for it.

METAMORA, Sept. 2d, 1846

Mr. CLARKSON — Dear Sir: -- You are about right in the time given to complete the repairs of the Canal, in the last American; but still I think we will be able to anticipate your time a little. We expect to have the new aqueduct across Big Cedar, ready for the water on Monday next, so that navigation (no more ill luck happening) can be resumed from Lawrenceburgh to Brookville, as soon thereafter as the Canal can be filled to Harrison — by the 19th inst. We hope to have a new aqueduct across Duck Creek at this place ready for the water by the 1st day of October, and possibly a few days earlier --we have as large a force engaged as can work to advantage, and are getting along very well. By the time boats can run below Brookville, we will have the break here so far repaired that we can fill the canal to this place, and start the Mills here, and have the canal navigable from Cambridge to Metamora.

The Canal Company having almost entirely taken off all tolls on articles transported on this part of the canal until the break is repaired, the shippers at Cambridge and Milton design to send their freight by canal to this place, thence by wagons to Brookville, and there again take the canal to Lawrenceburgh, or Cincinnati. The cost of these repairs will be from four to five thousand dollars.

Yours, Respectfully,
H.C. MOORE, Engineer.

10-2-1846

THE CANAL.

From the following letter from the energetic

Engineer of our Canal, we learn that the serious breach in the Canal at Metamora is repaired. This is good news:--

MATAMORA, Sept 30th, 1846

Mr. Clarkson, Dear Sir: -- I have the pleasure of informing you that the new Aqueduct at this place is now ready for the water, which will probably pass over it some time to night —at all events it will do so on to-morrow (the 1st of October.) I think the Canal will be filled to Brookville, and ready for navigation by Saturday afternoon, next. Yours Respectfully;

H.C. MOORE Engr.

10-7-1846

Laurel, &c. — On Friday and Saturday last we made a visit to Laurel and Connersville. We found the health of those towns pretty good. At Laurel they have but few cases of fever and ague, in comparison with other towns in the State, and but few in comparison with their town last year. The filthy bayou, which has heretofore engendered a large portion of the disease in the town, is now constantly washed with pure water. from some new mills just erected here.

Messrs. Shoup & Co. are erecting a splendid new flouring mill, at that place, to be propelled by water from the canal. The business of Laurel is rather dull at this time owing to the *injunction* upon the canal. But the mechanics appear busy, and cheerful.

Messrs. Macy & Co., have been building an addition to their Woolen factory. —They will start in a few weeks about 300 woolen spindles, and looms enough to make the yarn into cloth. Mr. Macy although he has been unfortunate, and had much to discourage him, yet he had that industry, and perseverance that will over come all difficulties. Such an establishment, with his real *operative* industry, adds as much wealth to a country as forty *speculative* trading establishments. — We want more of those establishments that with labor create wealth, and fewer of those establishments that live by speculating upon the labor of others.

10-16-1846

The Cincinnati & Cambridge DAILY LINE OF PACKETS ON THE WHITE WATER CANAL, HAS Now commenced running its regular trips; Leaving the White Water Canal Basin every day at 2 o'clock, P.M.; arriving at Cambridge next day in the afternoon, where passengers can always find horses and carriages in readiness, at Mr. Reiser's, to convey them to any point they may desire. Charges moderate, Returning, LEAVES CAMBRIDGE every morning, at 7 o'clock. Fare through, each way, \$3.00, including board. For passage apply to M'Clure & Dickey, White Water Canal Basin, Cincinnati.

C.S. ANDREWS & Co. Proprietors
Oct. 1846 42-2m

12-4-1846

Canal Trustees. — As there is a prospect of the objects of the Butler bill being accomplished, there are now several applicants for the offices of Trustees of the funds and canal. We suppose the Terre Haute dandy will be one of them. We have heard Michael G. Bright, John H. Bradley, and R.W. Thompson spoken of as candidates for those stations.

12-4-1846

The Packet Line — We do not wish our people to forget that there is a good daily line of packets on our canal. We have traveled several times on their boats, and find them regular, comfortable and accommodating. The proprietors, captains and hands, have made strong exertions to meet their engagements, and taking into view every impediment, they have done well. It is comfortable traveling on their boats, and we hope they will receive that patronage that will insure their continuance. They have been pretty punctual in their time, and as they get more customers are better provided against accidents, they must necessarily command the travel.

12-4-1846

The Cincinnati & Cambridge DAILY LINE OF PACKETS ON THE WHITE WATER CANAL, HAS, Now commenced running its regular trips; Leaving the White Water Canal Basin every day, at 2 o'clock, P.M.; arriving at Cambridge next day in the afternoon, where passengers can always find horses and carriages in readiness, at Mr. Reiser's, to convey them to any point they may desire. Charges moderate, Returning, LEAVES CAMBRIDGE every morning, at 7 o'clock. Fare through, each way \$3.00, including board. For passage apply to McClure and Dickey, White Water Canal Basin, Cincinnati. C. S. ANDREWS & Co. Proprietors
Oct. 1846

1847

1-7-1847

DESTRUCTIVE FLOOD

Jan. 1st 1847, will be a day long remembered in the White Water Valley. — And many years will elapse before the citizens of the Valley will be in as prosperous circumstance as they were on that morning.

It is our task to record the most disastrous flood, that has ever swept along the White Waters. It had rained for two days pervious to New-year's day, and the waters began to rise in our rivers on the morning of that day with fearful rapidity, and before midnight it swept almost every thing within reach of the river. It was higher by 10 or 11 feet than it has been for thirty-three years, and about three feet higher than it was at the memorable flood of 1813. It is almost useless to enumerate individual losses. Every farm on the river divested of fences,

THE WHITEWATER CANAL

and in many cases the soil. Mills and mill-dams are either ruined or seriously damaged. Roads are torn up, filled up with drift, or deep and impassible pits of water. And the White Water Canal is in ruins. The Southern part of Brookville, between the forks of the river, suffered most seriously. Some fifteen or twenty dwelling houses, with all the furniture, provisions and clothing of the occupants are gone, together with an innumerable number of stables, shops, out buildings, &c. &c. The new saw mill of Lynn and Adams is gone. —James White lost five houses. But the heaviest loss sustained in our town is the damage to the Mills of Messrs. Speer & Stephens. We have collected the following items of loss. This list is incomplete and may be in some instances over estimated, whilst in others it may be under the real loss. But in all cases there is a greater loss in detention of business. The following are some of the losses in this place.

Stephen Coffin	500
James White	3000
Wm. Robison	300
B. H. Burton	200
M. Shrunk	1000
Geo. Holland	500
Widow Pfaunder	300
Seneca Hall	100
A. Gagle	200
Joseph Rice	500
Lynn & Adams	1500
Adam Fogle	250
Z. Fogle	350
C. Fritz	50
M. Kaulenbach	500
Geo. Houk	500
R. Forrer	100
Danl. Forrer	50
J. Bowers	250
A. Bowers	100
Wm. Butler	2000
Geo. W. Kimble	1500
Tyner, McCarty & Roberts	200
Richard Tyner	900
Speer & Stephens	10,000
Samuel Goodwin	50
White Water Canal	10,000
County Bridge	1,300
Brackenbridge Estate	150
A. Mewhinney	500
John Teller	200
Peter Grimes	175
J. Woods	1,000
Geo. Widener	300
Bradly's Mill	1,000
John Roof	50
E. Sater	50
P. Spitzfaden	50
H. Klasner	50
J. S. Colescott	50
D. Keifer	150
Geo. Bohl	150
James Muriatt	100
Jefferson Tyner	100
Mr. Betzner	150
Mr. Smetzler	100

In the case of Speer & Stephens the loss is difficult to estimate. But be it what it may, it can under no circumstances injure their credit, their usefulness or their energy. They are already engaged with a strong force in repairing. They wish to contract with some safe, capable and persevering individual to repair their dam. They wish it done immediately.

The principal loss of R. Tyner is in fencing on his farm; but he is almost repaid in the great amount of rich deposits left on his farm, it being from 2 inches to a foot deep all over it.

The loss in this place fell principally upon the German part of the town. Many of them lost houses, furniture, clothing, provisions and lots.

But this loss has given our citizens an opportunity to manifest their charity.—And they have done it in a handsome style. So far as money, clothing provisions, comfortable houses, and kind words could restore the loss they have been supplied with a liberal hand. We have always loved the citizens of our village, but the liberal spirit and the deep sympathy they have manifested for the sufferings of others, attaches us more closely to the. We feel now that ourselves or families no matter what may be the reverses, the misfortunes, or the accidents that may befall us, we shall not suffer for the necessaries of life.

And in these acts of charities, our ladies have taken an efficient part. Although some of them delicate in health, regardless _____ (this part is missing)_____.

1-8-1847 DAMAGE TO THE CANAL.

It is impossible for us to specify the various breaches in our canal. Not more than one lock, (that at Harrison) is destroyed. But most of the locks on the line are washed round, which can be repaired by embankment, which is the cheapest of constructions. All the dams are remaining, but the water has washed round all of them, and formed a channel, which only requires cheap embankment. Three aqueducts, one across White Water at Laurel, one across Williams Creek, six miles above Laurel, and the other just below Cambridge, are gone. But they have not floated far, and much of the timber can be replaced. Where the canal was constructed on low bottoms and bluff sections, the water has made fearful inroads. The bridges at this place and at Harrison are safe.

The Brick Church, belonging to the Baptist, six miles above this place, on 6th West Fork, was entirely washed away on Friday.

Mr. Simmon's Barn, containing a large amount of grain, washed down and destroyed. His loss is very heavy.

At Cambridge, we learn that the damage is slight. R. & S. Tyner lost about 20 hogs. A few days after the flood, the pork and

warehouse of Mr. Lacky took fire and burned up. The loss in pork, flour, and wheat is said to be heavy.

Canal Election. —The annual election of Directors of the White Water Canal took place on Monday last. The old board was re-elected. We regret to learn that Samuel W. Parker's place in the company has been given to Miles Murphy of Henry Co. Mr. Parker has given life, energy, and credit to the company, and has made an efficient and popular President, and however active Mr. Murphy may be, he cannot expect to be as beneficial to the company. But Parker has a noble soul, and every good work will have his aid, counsel, and support, no matter what station he may hold.

We believe Mr. Parker would not consent to serve the company as President and attorney for the salary he received the past year.

1-8-1847

THE WHITE WATER VALLEY CANAL.

This work was completed and made navigable to Brookville in the fall of 1839. The State, a short time afterwards, expended one million, for the want of further means —leaving it unfinished to Cambridge City. The unfinished part of the work was about one-half completed — requiring some \$400,000 to prepare the line for navigation to the National Road. To accomplish this, the present Company was incorporated, upon condition of its completion within five years. The then prevailing sentiment of the masses was that the project was Utopian. The completion of the entire work, within three years from the date of incorporation — thereby affording an outlet for the abundant harvests of this great valley, and developing its unlimited resources is ample evidence of the ability and enterprise of this people, when their interests are involved and energies applied.

The flood that has just inundated our Valley —its length and breadth —spreading devastation in its track —has again presented our people a field for the re-application of those energies and abilities.

It is estimated from the best lights obtained, that the damage to the Canal from Cambridge to Lawrenceburgh, will not exceed *one hundred thousand dollars* —many affixing it as low as *fifty thousand*. Assuming the highest as the probable cost, the important question arises, how is the Company to command *one hundred thousand dollars* at this juncture? It is assumed as a "fixed fact" that the *directory* along must not be relied upon to meet this *crisis*. Those, interested through the Valley (and who is not?) must not fold their arms and look quietly on, but all should lend a helping hand, by their countenance and means.

That there has existed in this community deep rooted prejudice against the Company, growing out of their corporate acts, cannot be questioned, — that it is the duty and in-

THE WHITEWATER CANAL

terest of every good citizen, for a time, at least, to forget these animosities is as unquestionable. Assuming that about *one hundred thousand dollars* will make the repairs — it is thought that completing each way to the slack water of the Canal at Laurel and there re-shipping —the expense of re-building the aqueduct at that place may be deducted from the estimate of immediate repairs. Estimating this item at *twenty-five thousand dollars*, it will leave only *seventy-five thousand* necessary for immediate use. That this people will for a moment entertain the proposition of even a temporary suspension of the work, longer than is absolutely necessary to make the repairs, would seem incredible, when contemplating the innumerable and immense interests involved in its immediate repair. At the time the present company undertook its prosecution, everything was portentous of a failure —the county was just emerging from the general depression in its business affairs which succeeded the Bank suspension of 1837; the markets of the farmer were glutted —produce was extremely low, and the money market much depressed —how great the change! The currency, in a degree, restored —the debt of the county paid off —the demand for produce increased; and now, lieu of a constant drain from the producer, he is enabled to retain a share for future investment, and any emergency that may arise. It has arisen; the demand is upon him; and it is he that must ultimately bear the burden of the loss.

The new board of directors are now in session, deliberating, no doubt, upon the ways and means to speedily complete the necessary repairs. Let them issue bonds in denomination from \$5 to \$100, bearing interest from date until paid —call meetings of those interested throughout the Valley present the true situation of their affairs convince them of the Board's inability to act without further assistance; and that same energy and ability will mark their course that so signally developed itself in the completion of the heretofore unfinished portions of the work. — By This means at least \$20,000 might be obtained —add to this \$30,000 as a temporary loan from the Cincinnati, Lawrenceburgh and Richmond Banks, and already you have means nearly sufficient to secure a speedy navigation of the Canal.

It has been proposed in addition, to obtain from the several counties directly interested, pledges that they will each receive in payment of their county revenue, specified amounts of the paper of the Company —they issuing anew, an amount equal thereto and becoming responsible for its redemption with interest.

As the people of the Valley are most deeply interested in the early completion of the repairs —it is seriously hoped that with the funds that may be secured, the amount advanced for bonds, and any other means

that may be delivered to further the object, we may, in a short time, behold the resources of this great valley again developing themselves, and the wanted cheerfulness and prosperity of the people restored.

1-15-1847

The Canal Company

We learn that the new board of directors of this company, were organized last week, and adjourned to meet on Monday next, when it is supposed, they will adopt measures to commence at an early day, the repairs occasioned by the late freshet.

It is rumored that they intend applying to the present legislature, for the privilege of charging the repairs to the cost of construction. Should the legislature make such a provision, the company, no doubt, will issue additional stock to an amount necessary to make the repairs.

It is already widely known, that the stock is among the best in the State, and in all probability would, under the circumstances, be taken without much delay.

The available means thus secured, with that, as suggested last week, will enable the company, with proper exertion, to make the Canal navigable in a few months.

1-29-1847 Friday

White Water Canal

The Board of Directors for the White Water Valley Canal Company, convened in Connersville on Monday last. The first business coming up before the board was the appointment of officers for the coming year. John S. Newman, Esq., of Centreville, has been chosen president, salary fixed at 1,000; John W. Hitt, of Brookville, has been appointed Secretary, salary reduced from \$800 00 to \$600 00 per year, and —James Herron, of that place, was duly —elected Treasurer, and receives a salary of 400 00. Mr. Herron had the offer of the Secretaryship, but the duties of that office proves to be rather burthensome for his constitution; from this fact he declined the offer, when the board tendered him the responsible station of Treasurer.

There has been no further business of importance transacted. A measure has been adopted, however providing for the borrowing of money from individuals in large or small sums, the company giving bonds or notes for the same payable in two years. Money obtained in this manner will draw an interest of six per centum per annum.

No news having reached this place relative to what action the Legislature would take upon a petition sent up from a number of our citizens, praying for relief, etc. etc., and the engineer (Mr. Moore) having suddenly left this place, for, the Lord only knows where, and that too, just at a time when his services are most required, the Board has adjourned until the first Monday in February, when they hope to fall upon some definite plan for the immediate repair-

ing of the canal. —*Spirit of the Valley.*

2-2-1847

WHITE WATER VALLEY CANAL

Henry Valette vs the Whitewater Valley Canal Co. —At Chambers, before Judge McLean, September 25, 1847. Reported from the notes of the Judge.

Valette filed his bill to enforce a lien upon the Whitewater Valley Canal, and applied for a preliminary injunction to restrain the officers of the company from doing certain acts, prejudicial to his lien. — The application was fully argued by Mr. Fox and Mr. Chase for the complainants, and by Mr. J. S. Newman and Judge Walker for the respondents.

The following points were decided:

To enforce an equi lienable it is the appropriate jurisdiction of a Court of equity.

The Circuit Court takes jurisdiction for or against a corporation, from the place where its business is done.

And this sufficiently appears from the face of the act of incorporation.

The citizenship of persons who may or may not apply to be made parties need not be alleged in the bill.

The rights of persons not made parties cannot be affected directly by the proceeding in a suit; but a question which is raised between other parties may affect them, as the holder of certain paper.

A complainant may consent to the postponement of his lien in whole or in part, on condition beneficial to all the parties concerned.

But the Court cannot change a contract, under any exigency.

Judge McLean read the following Opinion:

This is an application for an injunction and the appointment of a receiver.

The bill alleges that on the 20th of January 1842, the Legislature of Indiana passed "An act to incorporate the Whitewater Valley Canal Company." With powers to make contracts, sue and be sued and do all things necessary to effectuate the objects of the association. And in the same act, the State transferred to the association all its right to the line of the Canal from the Ohio River to the National Road, at Cambridge, and all the expenditures by the State made thereon, on condition after the lapse of fifteen years and after the completion of the Canal by the Company, the State should have the right to resume the Canal, with the privileges granted, "Upon paying to said Company, the full amount of their expenditure upon the same."

Power is given to the corporation to borrow money, and when necessary to increase the stock of the Company—to erect Mills and other hydraulic works, to fix the rate of tolls, &C.

After the organization of the Company, by the election of its officers, they created a loan for the benefit of the Corporation of

THE WHITEWATER CANAL

one hundred and twenty-five thousand dollars, and for part thereof issued bonds for the payment of one thousand dollars each, one hundred and twelve of which bonds bearing date, 1st of January 1845, were issued and delivered to the complainant payable to him or bearer. On these bonds, interest at the rate of seven per cent, was payable semi-annually, at the city of New York; until the payment of the principal sum; which was payable in ten years, being part of the first and only loan of one hundred and twenty-five thousand dollars, and the faith of the Company and their effects, both real and personal, were inevitably pledged, and said bonds were to have a preference over all debts that might thereafter be created by the Company.

And in default of said payment it was agreed that the holders of the bonds might enter immediately into the receipt of the tolls and water rents, and the incomes of said company, by applying to the Circuit or district Court of the United States or any court of justice, to appoint a receiver of the incomes of said company and apply the proceeds in the payment of the interest on said bonds, &c. And it was agreed that should the interest have to be collected by legal process, there should be adjudged to the holder ten per cent, as liquidated damages.

The company also executed to the complainant four other bonds of similar character and amount, except as to their date and time of payment. Two of these bonds are now due and the other two will be due in July next. That there is now due the sum of five thousand seven hundred and fifty dollars for interest, also one thousand dollars on the obligation which became due in July 1847, which it seems the defendants refuse to pay —And the complainant states that the corporation has, within a few months, contracted debts, and has in violation of law, caused about seventy thousand dollars in bonds to be prepared to be issued and has issued about twenty thousand dollars of the same, and threatens to issue the balance thereof, for the purpose of being used as a circulating medium and as a substitute for bank notes in the form of promissory notes by which said company promises to pay two years after date to _____ or bearer five dollars, (and other notes from that sum in twenty dollars,) for value received, with interest at the rate of six per cent, per annum; and which notes on their face are agreed to be received by said company, at all times for tolls and water rents, &c.

And the complainant avers that the corporation has lands and personal property, debts due and cash on hand to a large amount. That the corporation owes, as has been informed, over two hundred thousand dollars. That if the said notes shall be received in payment of tolls and water rents the sum due to the complainant as afore-

said, cannot be paid; and he prays that the defendant may be enjoined from settling or disposing of any of the real or personal estate of the company, and from issuing or circulating any promissory notes of the character before described. Also, from receiving them for the tolls and water rents due and to come due.

In its answer the corporation admits the organization of the company, the issuing of the bonds and the sum due to the complainant, as alleged by him. It states that by a great rise of water in White river an extensive injury was done to the canal to repair which ninety thousand dollars were required. That these injuries if not speedily repaired would have been ruinous to the canal. That failing to raise funds to make the repairs in any other manner, the plan of issuing the promissory notes complained of was adopted. These notes the defendants insists are not in violation of law.

Several objections were made to the jurisdiction of the Court.

1. That there is a remedy as law.

This is an equitable mortgage, and is a peculiar subject of equity. The objects of the complainant are clearly not attainable at law. He may recover a judgment against the corporation, but its tolls and water rents cannot be reached in that form. And it appears from the face of the contract these were looked to by the parties as means of payment. This remedy is incorporated into the contract, and it is a part of it. On the tolls and water rents, therefore, the plaintiff has a lien preferable to all others now shown, which may be enforced in a court of equity, but cannot be in a court of law. And this is the main object of the bill.

2. It is also insisted that it does not sufficiently appear that the place where the corporation does its business is within the State Of Indiana. To this it must be answered that the place where the functions of this corporation are discharged must, necessarily, be within the State of Indiana. It can exercise no extra-territorial power on this subject. — But from the face of the charter, it is seen that the work to be accomplished is within the limits of the State.

3. It is further objected, that the complainant was for himself and other interested, and that it does not appear who those persons are, and that some, or all of them, may be citizens of Indiana, who could not come in as co-plaintiffs. If this supposition be true, it would be a sufficient objection to their being made plaintiffs. —They are not now plaintiffs, and this objection may be considered when they shall apply to be made so.

4. Again it is insisted that the rights of the holders of the promissory notes alleged to be illegal are involved, and that they should be made parties.

So far as the question of illegality is concerned, it is not material that they should be named as parties. Whether these

notes be in violation of law is distinctly presented by the prayer of the bill that the corporation should be enjoined from issuing any further notes of similar character, which they have no power to do. As well might it be objected, when a defense is made involving the legality of a promissory note, that the rights of other holding similar notes would be effected. If these notes, now in circulation, are to be treated as valid, and the question is made whether the payment of them out of the tolls and water rents, or which is the same thing in effect, whether they shall be received in a payment for tolls and rents, as — pledged upon their face, the objection that the holders are not made parties, is not without force. In this aspect, the question is one of preference and that point is not raised in the bill; and it is supposed could not be, unless the holders of the paper were parties.

5. The State of Indiana is not a necessary party. Its interest is contingent, depending upon the exercise of that discretion. And this proceeding can in no respect affect the exercise of that discretion.

The lien of the plaintiff is the first one, as appears from the bonds, and it was expressly agreed, that it should be preferred to all order. But, of necessity, there was an implied understanding that the ordinary expense of the company should first be paid. Until this was done there could be not tolls or water rents to pay out. But this expenditure is limited to ordinary repairs and other expenses, incident to the business of the company. The Directors could give no lien, to the prejudice of the plaintiff, beyond this. The work was subject to casualties like other and similar works, but no provision was made for extraordinary expenditures. — When these became necessary, as under the circumstances states in the answer, the Directors should meet them, if possible, by the use of other means, than those which were mortgaged to the plaintiff. —They had lands, debts due for stock and other wise and they had power to increase the stock of the company. If these should not be available, after a full trial, and a pledge of the receipts for tolls and water rents was the only means to raise money to make the repairs, within the power of the Directors. It was a subject rather of compromise between them and the complainant, than of legal discretion on their part. The lien may have been given to the complainant, injudiciously, but it was given under an emergency as strong and indeed stronger than that which now exists. The means afforded by the plaintiff enabled the Company to accomplish the enterprise. The lien given to him induced him to part with his money, and no change of circumstances in the affairs of the Company can authorize a postponement of the lien.

The interest of the parties in this case is identical. Unless the Canal be repaired, the expenditures of the company will be

THE WHITEWATER CANAL

lost, and the work in a short time must become of little or no value. And in this event the claim of the plaintiff must fall and become as worthless as the stock of the Company. It would seem therefore, in reason and policy, that the future receipts of the company should be used to make the repairs now being made, so far as may be done with a proper regard in the interest of the plaintiff. And he voluntarily proposed to postpone his lien for ninety days, provided the receipts of tolls and water rents, during the ninety days, shall be applied to the completion of the repairs. And after the expiration of the ninety days, he consents to receive one-fourth of the receipts for toll and water rents.

Beyond the ordinary repairs of the Canal and the expenses of the Company, it can create no demand, which shall either directly or indirectly postpone the lien of the complainant. The faith of the Company is not only pledged for the priority of his lien, but its entire property and especially the receipts for tolls and water rents. The tolls and water rents are not only mortgaged equitable, but they are set apart as the means of payment. This being the contract the Company cannot change it, nor can the Court do so. Courts of equity do not make contracts, but enforce them. As the complainant however, has consented to the postponement of his lien, as above stated, that all means of the Company may be applied to the repairs now being made, all difficulty on this point is obviated.

The rate of the lands of the Company for its stock, lessens so much the property mortgaged to the complainant —the lien extended equally to the stock and the land, though the stock was held by individuals —the exchange of the land there for stock, did not add to the amount of stock, but reduced the subject of the lien, to the amount of the land sold. This, the plaintiff may object to, as it lessens his security.

The promissory notes of small denominations printed on bank paper, and containing a promise to pay, with interest, a certain sum, and receivable for tolls and water rents; signed by the President and Secretary of the company, being evidently intended for circulation, are clearly within the act of Indiana of the 26th of January, 1841.

Upon the whole I think the complainant is entitled to the prayer of his bill to enjoin the corporation from issuing notes of the denomination above stated, and from receiving such notes already issued in payment of tolls, water rents, or other dues; also from selling any of the real estate now held by the corporation for its stock. And after the canal from Cambridge to the Ohio river shall have been in operation from this time, three months, the receipts having been faithfully applied to the completion of the necessary repairs, the company is required to set apart one fourth of the receipts for the payment of the plaintiff's de-

mand; and that the same shall be paid to the plaintiff or deposited in the Lafayette Bank of Cincinnati, subject to the order of the Court.

And it is further ordered that the corporation shall, by its proper officer or officers, make a report to the next Circuit Court of the United States, to be held in the State of Indiana, stating the receipts and expenditures of the Company from the time this injunction is allowed up to that term, and that another report of the same character shall be made at the succeeding term of said court, if the plaintiff's demand shall not be discharged before that time.

A receiver will not now be appointed. — And I take occasion here to remark that I have no doubt the Company has acted under the exigencies in which it was placed, with an increased desire to advance the general interests of the association and the public.

2-6-1847

WHITE WATER CANAL.

The Legislature, before its adjournment, passed a law loaning to the White Water Canal the State revenue of the counties of Franklin, Fayette, Wayne, Rush and Henry, amounting, in all, to about \$50,000. This would have put in the hands of the company sufficient ready means to have insured a speedy repair of the canal. But Whitcomb put the bill in his pocket and refused to return it either with or without his signature. The Legislature having adjourned before the elapse of five days the bill is defeated.

Of this act, it is useless to speak. There has been one long continued and bitter curse uttered from the whole length and breadth of this Valley, by both parties, upon that tyrannical scoundrel and villainous despot —JAMES WHITCOMB! His acts forbid our suspecting him as a private man or as the executive.

2-6-1847

CANAL MEETING.

On Tuesday evening last, after the adjournment of the Turnpike meeting those present organized to give some expression relative to our Senator and Representatives, and the action of the Governor on the loan to the White Water Canal Company. A resolution was introduced censuring the Governor for retaining the bill, which was warmly discussed by Messrs. Ryman, Berry, Howland, Jones, Kilgore, &c. when the meeting adjourned until the next evening.

On Wednesday evening the meeting was addressed by Messrs. Ryman, Parker, Jones, Farquhar, and the resolutions having been amended, were unanimously adopted, as follows:

Resolved, That the thanks of this meeting be rendered to Dr. Geo. Berry our Senator, and Dr. E.D. Crookshank and Spencer Wiley our Representatives in the Legislature, for their successful exertions in obtaining

the passage of a law authorizing a loan of \$50,000 from the State to White Water Valley Canal Company, to repair the damage done to the Canal by the recent disastrous freshet.

Resolved, That James Whitcomb, Governor of the State of Indiana, in his unjustifiable and uncalled for refusal to return to the House, whence it originated, that it might become a law, the act of the present session of the Legislature, authorizing a loan to the White Water Canal Company of an amount of cash means equal to the present year's State revenue, arising from the five counties contiguous thereto and to enable them to make the repairs occasioned by the late disastrous flood —merits our unqualified disapprobation and severe condemnation.

And the proceedings were ordered to be published.

2-6-1847

WHITE WATER CANAL.

We understand that John W. Hitt declines serving the White Water Canal Company as Secretary at the reduced salary of \$600. The Company, as well as States, or Corporations never gain anything by reducing salaries so as to drive the best men into private life.

The Company have elected John Newman as President at a salary of \$1000. — He is a good man, none more worthy of confidence and esteem —none more safe and competent in ordinary times. — But he is not the man for the present crisis. He is not the man to galvanize the dead carcass, and infuse life, energy and success into the people and resources of this great Valley. It needs a mast spirit, and almost superhuman power to inspire, collect the means, and carry through this great work. The board has been in session at Connersville this week. We have been patiently waiting the result of their deliberations. We wish to know their plans and prospects. Let them prepare, and we are at least ready to blow for them. At present we know not what to say. The canal must be repaired; but when or how, we leave to those appointed for that purpose. Those who shoulder this vast undertaking will have all our sympathy, for it is no common task.

2-12-1847

The Canal.

The Board convened on last Monday. Some business of importance was transacted. An order was passed authorizing the issue of \$10,000 in bonds payable in one, two, and three years, to the Millers of Lawrenceburgh, who proposed to furnish a considerable sum of money to repair the feeder dam at Harrison, and the canal from thence down. The water-rents and tolls on that part of the canal to be pledged for security. It is to be hoped that the mill owners will accept the bonds and *fork over* the dimes.

A similar order was passed authorizing

THE WHITEWATER CANAL

the mill owners, having water privileges along that part of the canal situated at and between Laurel and Brookville, to repair the same; the Company pledging the water-rents between those points for the payment of the bonds. The cost will be about \$7,000.

The Board also passed an order appointing Mr. J. W. Macy superintendent of re_____ (this part is missing)__. We now seize the opportunity of recommending to the favorable consideration of our citizens, the necessity and propriety of granting individual loans to the Company for the space of one, two, or three years, taking bonds, &c. as security, and thereby enable said Company to proceed as soon as possible to repair said canal.

We have been informed that John W. Hitt, lately elected Secretary of the Board, refused to accept the office on the ground that the salary of \$600 was insufficient. The Board, at its late session, re-elected Dr. L.B. Tupper, and fixed his salary at \$700. A very good appointment. Mr. Tupper's qualifications to fill that office are too well known to admit comment. *Spirit of the Valley.*

5-7-1847

The Canal. — Wilcox and Calvin have a strong force on the canal between this place and Harrison. But they have cause to complain of the farmers along the line, for the illiberal course they are disposed to pursue. They are asking exorbitant prices for their produce, because it has to be paid for in the individual notes which the company are compelled to use. There is no use or reason in this. These notes are good. They will purchase merchandise of almost any kind, as cheap as cash. Tyner wants them and many others. We want \$5000 of them, and if a good many will pay us their newspaper accounts we will discount on their bills, instead of discounting their money.

Come neighbors, let us manifest a little liberality, and give these notes a good credit. We can do it, and if we do not do it, we are unworthy the country in which we live, the pure air of heaven we breathe, and the choice blessing Providence is showering upon us.

6-11-1847

The Canal -- We are told that the water will again be in the canal as far as this place sometime next week. This will give us the benefit of the water for the machinery and that is about all. It is impossible to see when it will be repaired below here —not perhaps before September.

Being once thus deprived of the canal we are reminded of what we once were, and will hereafter more fully appreciate its benefits.

7-9-1847

The Canal. -- The water is now in the Canal,

from Cambridge to this place. From here to Cincinnati, the repairs are progressing rapidly, but we are not prepared to set the day of its completion. Deeply have we suffered for the want of this Canal.

We understand that the directors have determined to erect a gate, and make the bridge across the river at this place, a toll bridge. At this no reasonable man can complain. It belongs to the company, and if they keep it in repair for the use of the traveling public, the company should not pay the expenses. Let us at least be reasonable, and let not our self-interest too much control our judgment.

8-28-1847

THE CANAL. — We conversed with Mr. Moore, the Engineer on our canal, on Wednesday last. He is of the opinion that water will be let into our canal, the whole length, by the 1st of September.

The Madison and Indianapolis Rail Road will also be finished by that time.

The Wabash and Erie Canal has lately been let out to contractors, from Coal Creek to Terre Haute.

8-28-1847

Halstead's Mill. —The damage claim of R.W. Halstead against the W.W.V. Canal Company for damage to his mill by the abstraction of water, was determined in our Circuit Court on Friday last. He recovered a judgment for the pitiful sum of \$10.50.

8-28-1847

Our Canal. — It is said that Valette is about commencing suit against the W.W.V. Canal in the District Court of the United States to recover their indebtedness to him, and to have a receiver appointed to receive the tolls for him, thereby refusing to receive the notes and obligations put in circulation to repair the Canal. He may succeed in such a scheme, but it he can prosper in such acts, with the righteous indignation of the people, and that God who is not unmindful of the acts of the oppressor, against him, he may pursue his own course.

9-24-1847

Recovered. — Mr. Burton has recovered the Canal Certificates, &c., which were taken from his store on the night of the 14th inst. They were found on Saturday last, near the canal, two miles above town. The pocket book, all the papers, the bags in which the silver was taken, and everything that could be positively identified, had been left. But the burglars kept all the money and bank notes. It is evident they left those certificates there, after they were advertised.

10-8-1847

The Canal again. — From the boatmen we learn that water was let, at Harrison, into the Cincinnati branch of the White Water Canal, and that navigation will open to Cin-

cinnati, early in next week. — This is a matter of rejoicing to the people of the White Water Valley. It not only opens a market to our vast resources, but it is the triumph of effort, which, on the first of January last, appalled the stoutest hearts.

10-22-1847

The Canal. — On our first page will be found the decision of judge McLane, in the case of Valette vs the W. W. Canal Company. His decision prohibits the company from re-deeming any of the 5 or 50 dollar bonds issued this summer for the purpose of repairing the Canal. The citizens all along the line have promised to pay certain sums of the 1st of Jan., for which they have given their notes in denominations of five dollars, for which they are to receive canal bonds, receivable for tolls and water rents. But this decision prevents the company from receiving them. The consequence is many of the citizens will refuse to pay their bonds, as the company cannot comply with her part of the contract, thus creating endless trouble and litigation, and great loss to the poor laborer, in whose hands many of these individual notes now are, having received them in payment for labor. This too, all grows out of the insatiate, vampire appetite of money brokers.

11-12-1847

DESTRUCTIVE FLOOD.

Again have the citizens of the White Water Valley been compelled to suffer loss by another destructive flood. Not having yet recovered from the effects of the flood which laid waste our valley on the 1st day of January last, on Monday night last the same scene was enacted again. — From heavy and continued rains the White Water rose to an alarming and destructive stage, and has destroyed the canal, crops, mills, stock, lumber, fences, and every thing on the low bottoms from Cambridge and Richmond, to its mouth. We are literally in wreck and ruins.

The canal, which had required all the means and energy of our valley to repair, and which had only been navigated one week, is now in ruins, when or how to be repaired, we cannot say. All the repairs at the dams at Harrison, Case's, and at Brookville, are torn away, and worse, if possible than before.

From Brookville to Laurel, we suppose, from the best information, the repairs would not cost over \$1000. It is presumed therefore that the repair of this last name section will be done immediately, for the purpose of accommodating the mills and machinery.

The water in the main river, below the fork rose to within 8 to 12 inches of the flood of the 1st of January. But the water in the East Fork was two feet lower, and in the West Fork two feet two inches lower. The reason it was high in proportion below the forks than above, is this: In Jan. last the

THE WHITEWATER CANAL

East Fork rose and had fallen four or five feet, before the West Fork was at the highest. But on Monday night, both rivers rose at once, and poured their greatest force into the main river at the same time.

We are, however, disposed to believe that the damage is as great now as in January last. The crops of corn were un-gathered, and thousands of acres are entirely washed away.

In this place the loss is not so great, as no houses have washed away. Yet many of our citizens have lost heavily. The embankment of Speer & Stephens' mill is gone, together with some 200 cords of wood. The Water was also four feet deep in their paper mill, destroying much stock, &c. Mr. J. Woods, estimates his loss in cotton and machinery, at \$1000, besides the stoppage of his factory. All the mills and factories are much injured. Mr. Pelsor, who is building a new mill and dam just above town, on the East Fork, had a large part of his dam taken away. Mr. John Shafer has lost considerable in the destruction of his tannery. But the farmers from the head to the mouth have lost fences and crops, to an amount fearful to estimate; and it falls more heavily, as they had no recovered from the last.

The water on Monday night last was at least two feet higher than at the memorable flood of 1813, and some six or eight higher than any other except last January. The small streams in this neighborhood were much higher than in January, and we understand have been very destructive to mills. We learn that Wright's _____ a flood, has taken us nine months to repair so as to navigate, but no much more than half repaired. But now we are thrown back again as far as ever, with our engines prostrated, and with less hope for the future. To build fair hopes for the future, would be assuming what we do not feel. Yet this valley cannot, and will not, do without some public, convenient, and speedy means of getting their surplus to market.

Later -- We learn that the river was not as high at Connersville by 5 feet as it was on the 1st of January. The damage to the Canal above Laurel is represented as slight. At Laurel a warehouse was taken away, together with a large quantity of flour belonging to Mr. Hollingsworth --- some say he lost 500 bbls. of flour. A Canal boat belonging to Messrs. White and Riggs of this place, broke from its moorings at Laurel, passed over the dam, and lodged some two miles below. We suppose it is a total loss, of about \$1000.

At the dam, two miles below Brookville, all the new work is gone, together with the Canal Lock, and about 73 feet of the old dam.

At Harrison the river was within six inches of the rise in January, showing that the rains were heaviest in this region -- The old dam there is safe; but all the embankments made by the Cincinnati Co. and by

the White W. W.C. Company, are all taken. We still continue to hear of many heavy individual losses.

11-19-1847

Speer and Stephens Mills, will be in operation again in a few days. The repair of the dam was let to Messrs. Wilcox and Reed, who commenced operations on Tuesday last. If the Canal Company had half the energy of these men, the canal would be repaired in six weeks. The canal has three dams to repair, each of which is about equal to the dam of Speer and Stephens. Yet the company expect to be eight months repairing. The canal belongs to probably 100 men, five of whom are as capable of repairing the canal as these men are their dam.

Will the Canal be repaired? -- We understand that it is the determination of the Board of Directors to repair the canal --But will they do it now, or will they wait until the tolls that might be collected in the time spent in talking, would do the repairs? The quicker it is done, the easier and the better. The public is tardy in helping or trusting an individual or company that lacks energy or decision. Every day they remain inactive sinks them deeper in the mire. Twelve per cent in cash, upon the capital stock, in good hands, would repair the canal in three months. The dams are not worse than Speer & Stephens'. They will have theirs done in a few weeks, whilst the company are talking of twice as many months.

The Canal -- Capt. Woods, has a heavy force repairing the canal on the level opposite our town. He is pushing forward the work with that energy he usually applies to his own business. If the weather be favorable, he will be ready for the water early next week.

Messrs. Tyner & Roberts are repairing the balance to Metamora. The fact that they have charge of it, will insure its speedy completion.

We learn that the water is in the canal to Metamora, and the mills at that place are running. Before the close of another week, we think the mills in this place will begin to operate. This is doing things up right.

The canal, therefore, in a few days, will be in operation from Cambridge to this place. We will therefore, for some months be at the foot of navigation, and produce from here will have to be wagoned.

11-26-1847

THE CANAL.

Since the late flood, the Board of Directors of the White Water Valley Canal have convened, talked over the probably price of pork, and adjourned, to meet again on yesterday or to-day. This will be about the result for three or four months, which they will conclude to do nothing. -- It is estimated that the damages by the late flood is about \$30,000. This, with an energetic and

business board would be but a slight and temporary embarrassment.

We feel as free to speak of the lack of energy in one man, as we do to applaud the hobbler efforts of others. Notwithstanding some of the present Board are good Directors, and are all good men in their appropriate places, yet the public never had much confidence in them to fill the places where it required minds to plan and action and decision to prosecute.

But we do sincerely wish they would either do something soon, or at once acknowledge that they are not calculated for Directors, resign and let their places be filled by business men. We have not a word to say about their honesty or their capability to perform the common business of life such as raising corn and fattening hogs.

We could select a board, who would command the confidence of the public; and by the boldness of their strokes and energetic action, would electrify this whole valley, and the tracks of the destroyer would vanish, and business and prosperity would again smile upon us.

If the board were composed of such men as Samuel W. Parker, M. Helm, Geo. G. Shoup, Griffin Taylor, John D. Jones, Benj. Conklin, James Rariden, James Woods, and R. Tyner, in four months from this date the richly laden boats would float down our valley --the laborer on the canal would receive the pay for his hire, and the public would feel assured that all was right. This at least is our opinion, and we have as much right to it as any one.

We have no confidence that the present board will ever repair the canal, neither have the public or the stockholders. Then when shall we expect a different administration?

12-17-1847

The Canal. --The water was let into the canal at Metamora on Tuesday last, as it is now full to this place. The mills therefore, will be in operation. The canal is now navigable from Cambridge to this place, with the exception of some quarter of a mile at Laurel.

For this _____ (several unreadable lines) _____ company that what they have expended in these repairs will ever be refunded, even in water rents.

1848

1-7-1848

Tyner & Roberts are retailing flour at 4.25. The mills here are in operation, but there was much wheat purchased before the canal broke that they are well supplied. No sale for corn except for present consumption.

There are many hogs yet along our rivers, which will have to seek a market at Cincinnati.

Localities.

Rev. Ludlow D. Potter of Penn. Has

THE WHITEWATER CANAL

been engaged as Pastor of the Presbyterian Church in Brookville.

Messrs. Powell & St. John have opened a new dry goods establishment in our place.

There is to be another Masonic Lodge instituted in Brookville shortly.

Mr. Wm. Bradley, having sold his lease on Kimble's mill, has removed to Cincinnati, and rented a steam flouring mill.

Mr. Haymond, of Cincinnati, has taken the lease on Kimble's flouring mill. It is now about ready to start, taking water from the canal.

Our members of the Legislature have departed again for the seat of Government.

1-7-1848

White Water Canal Directors.

The election for Directors for the White Water Valley Canal, took place at Connersville on Monday inst., and resulted in the election of the following gentlemen:

S. W. Parker, John Newman, Robt. Murphy, Geo. G. Shoup, A. B. Conwell, John Godly, Caleb Jackson, Geo. Fryberger, and Mr. Davis.

This is a pretty good board. We presume by this time they have selected a President. If they have selected Mr. Parker, and will give him enough to warrant his abandoning his private business, and throw the entire energy of his soul into the cause of the canal, he will, aided by the board, repair the work in a few months. Without his energy and influence, the canal would not have been dug, and with out his aid, the present stockholders will have to forfeit every thing they had paid.

1-28-1848

W. W. Valley Canal.

CONNERSVILLE, Jan. 25, 1848

The Board of Directors of the W.W.V. Canal are now in session in this place. The Legislature having passed an amendment to their charter allowing them the privilege of issuing Company Bonds, it is understood to be the intention of the board to issue \$100,030 of bonds, and sell them to the stock-holders in proportion to their stock at 50 cents to the dollar. In this way they anticipate raising some \$40,000 in a few weeks.

In conversation with one of the board, we learned that it is the intention to advertise lettings in a few days—and that contracts for the entire repair will be made by the first of Marsh, with the intention of having the canal navigable in three or four months from the 1st of March. None of the board doubt that the canal will be repaired by August — some think sooner.

The State has entirely and forever surrendered her interest in the canal, which makes the stock look up a little. John S. Newman is again President of the board.

The Circuit Court, which has been in extra session in this place, adjourned on Saturday last. During its sitting Elias Macy

vs Clum was compromised, after a laborious investigation, and which the jury was out deliberating upon it. We understand that it was settled amicably, and the parties returned home good friends. Clum retracts all he had said derogatory to Macy's character —acknowledged that he had been deceived and misled into the expressions he had made, and that he now had every reason to believe Macy a gentleman in all his acts and conduct. Each one is to pay half of the costs.

There have been only about 4000 hogs packed in this place this winter. The destruction of the canal is ruinous to the business of the town, and the further we retreat from Cincinnati, our great mart, the more deeply is it felt.

Mr. Beard has resumed his old stand as landlord, where we advise our friends to call.

As we have never published the result of the election for Directors of the W.W.V. Canal, in exact figures, it is hereto attached:

C. B. Jackson,	3033
G. Fryberger,	2394
G. G. Shoup,	2391
John Godly,	1800
J. S. Newman,	1699
Robert Murphy,	1694
A. B. Conwell,	1671
G. Davis,	1671
S. W. Parker,	1664
J. Rariden,	1596
H. Mount,	1498
R. Tyner,	1448
J. Woods,	1429
H. Valette,	1408
M. Helm,	1317

There were two tickets run. The Newman Ticket, and the Parker Ticket. Jackson, Fryberger, and Shoup, were run on both Tickets. Five of the former were elected, and four of the latter, leaving Parker in the minority. He therefore, has no influence on the board, and is not in the least responsible for its actions. C.

2-4-1848

White Water Valley Canal

The Legislature about two weeks since, passed a bill conferring in the White Water Valley Canal Company the power to issue bonds for repair of the canal and surrendering all rights, interest and control of the State is the Company forever. This bill the Governor vetoed. We have not yet seen the bill or the veto of the Governor, but we find in the proceedings of the Senate, of the 27th ult., the following proceedings, to wit: "Mr. Murray moved to take up the message of the Governor, returning a bill to a amend and act entitled an act to incorporate the White Water Valley canal company, with his reasons for vetoing the same, which carried by consent. —The bill provided for the issuing of bonds for the purpose of repairing the damages done to the canal by the high wa-

ters.

A call of the Senate was had, and all the Senators being in their places, Mr. Walpole said that he was opposed to the passage of the bill, and had he been present at the time of its passage, would have voted against it, not for the reasons assigned by the Governor, but because it gave power to issue an unlimited quantity of scrip, for the bonds were nothing more than scrip, and he had seen the consequences of flooding the country with irredeemable paper. If bonds must be issued, he wanted the issues restricted in amount and the proper guards thrown around them so as to prevent the holders of such bonds from losing and being swindled.

Mr. Murphy said that he did not intend to debate the question on the merits of the bill. —But he thought that Mr. Walpole was somewhat mistaken in his views of the provisions of the bill. It did restrict the amount of bonds to be issued in what was necessary to repair the canal, and it was expressly provided that they should not be used as a circulating medium. —They were stock bonds to be sold to purchasers.

Mr. Holloway sustained the bill in some remarks, giving a statement of the situation of the work. He regretted that the governor against so large a majority as appeared in favor of the bill, had thought proper to veto it, for he thought that without the passage of the bill, or one similar in its provisions, the canal could not be repaired, and it was important to a large portion of eastern Indiana, that the work should be repaired so as to give them facilities from transporting their large amount of surplus produce to an advantageous market: he hoped the bill would pass.

Mr. McCarty said that he was very desirous that the canal should be repaired, and its usefulness restored. He had opposed the amendment by which the interest of the State reserved by the original grant was to be surrendered to the corporation. He had discovered a too liberal spirit in the grant of corporate privileges. A provision by which the profits over 8 per cent on the money expended by the company, are to be paid into the State treasury was repeated. This was a virtual surrender of over a million of public money to these corporators. He was opposed to it also because it removed restraint from the company in regard to tolls. They might make the tolls exorbitant —this left solely to the company. If it was desired it might be exercised. And knowing that the wealth of the company cannot be increased except at the expense of the people, he would not consent that the people of the valley should be left at the mercy of the company. The dividends might at some so very distant day amount to 50 per cent on the amount expended, and he wanted to have the State have some benefit for the money she had expended if such should be the case. He had other objections but in his

THE WHITEWATER CANAL

anxiety to give all needful power to the company he waived minor objections.

Mr. Miliikin thought that Mr. McCarty came here with feelings hostile to the interest of the company, from the fact of his having been employed in some suits growing out of difficulties between citizens of his town and the company. He thought that the work was one of importance and that in order to have it repaired, the State should extend a fostering hand and assist in placing it again in order; if it can be done without prejudicing the interests of the State. If the work was to be as profitable as suggested by Mr. McCarty, the stock would soon, instead of being worth 35 or 40 cents on the dollar, its present worth, command a premium, and the right being reserved to the State to take back the work, she could at any time when to her interest so to do, take the work and dispose of it to another company, thus receiving back the eleven hundred thousand dollars, she expended on the work and a handsome profit besides. — He was anxious to see the influence of the executive on this floor; 7 Senators voted against the bill, and he was anxious to see how many would at the beck of the Governor change their positions on the question. The objection taken that there was no security provided for the payment of the bonds was not valid. The purchasers would secure themselves in this matter, they were not generally blind to their own interests, and would he had no doubt before they purchased make the proper provisions in this matter.

Mr. Ellis thought that there was no danger of the bonds becoming a circulating medium, and he thought there was sufficient restriction in regard to the amount of bonds to be issued, and if they issued more than the necessary amount to repair the canal, they could at anytime be restrained in the proper court of justice for violating the provisions of the act.

Mr. Winchell said that he lived far distant from the work but he felt it his duty to assist the citizens of that part of the country. They asked no loan on the State of Indiana, but to be allowed to make an issue of bonds to bring into activity useless capital. There was no danger of the country being flooded with bonds for it was against the interest of the company to issue more than was necessary for it had a tendency to depreciate the value of the stock.

The question was then put on the passage of the bill, and decided in the negative, ayes 54, noes 26.

2-4-1848

W. W. Valley Canal.

All the fine proposals for a speedy repair of the White Water Valley Canal, as set forth in our last paper, were suddenly blasted. The bill passed by the Legislature, authorizing them to issue bonds, was vetoed by the Governor.

Public Documents.

We are indebted to the Hons. John L. Robinson and C. B. Smith of Indiana, And Stewart of Penn., and Reverdy Johnson of Mdl, for various public documents papers, &c. &c.

We are also indebted to A. B. Line, Esq., of the Indiana Legislature, for various public documents, who which he will receive our thanks.

White Water Navigation

Within the last few weeks, Mr. Remy of this place, has been engaged in boating pork, lard, and flour, from this place to Harrison, by the river. His boats are cheaply and lightly constructed, with flat bottoms, so as to carry from 20-25 bbls. Of pork. There being no dams, they float down in a few hours safely. The boats are so light that two horses will haul back three or four of them on a wagon. The wagon, therefore, takes down a load of flour and brings back a load of boats. Mr. Remy is a genius, and if he had ample means, would make a show in the world.

Flouring Mill.

Messrs. Armitage & Haytman have started one run of stone in Kimble's Mill. It has been in operation three or four weeks. This one run manufactures 16 bbls. of flour in three hours, out of 69 bushels and 20 lbs. of wheat. The miller says he can make 125 lbs. with one run of stones in 24 hours. They are preparing four run in his mill, and will have them in operation in a few months, when they will, at this rate, manufacture 500 bbls. per day. It is the intention, however, to have only three run in operation at once.

This is a little better than any mill of which we ever heard. The mills at Rochester and Oswego, New York, which we visited last summer, seldom ever make over 100 bbls. to a run.

2-4-1848

Canal Trustee.

Austin M. Puett, of Parke co., has been elected a Trustee of the Wabash and Erie Canal. We rejoice at his election. He is a gentleman of talents and integrity, and will make a good Trustee. He is a democrat, but that has nothing to do with his office, and we therefore rejoice at every instance, in which even a few have independence enough to break over the trammels of party. The reign of political slavery has lasted long enough, and we congratulate Mr. Cookerly and other, upon the moral firmness that have manifested in resisting its iron edicts. Party has nothing to do with his office; yet those who adhere to party for the purpose of power and place, would make the world believe they have committed an unpardonable sin.

2-4-1848

Williams' Creek Aqueduct.

The canal aqueduct across Williams'

creek, six miles above Laurel, fell off the abutments last week, which will stop the mills at Laurel. The damage thus done is about \$1500, but the delay and the consequent loss to the mills, is still more.

2-18-1848

The White Water Canal.

The Board of Directors were in session at Connersville on last week. The bill lately passed authorizing them to tax the stock to the amount of \$5 to the share, was before them. —The President of the Board —who is a very modest man —doubting the constitutionality of the law —hesitated until nothing was done, and the board dispersed without any definite action.

We are not disappointed. The board is yet inefficient. They have not the energy and harmony necessary for this or any other great enterprise. But if they have not sufficient sense and energy to do any thing, we should think they had sufficient HONESTY to acknowledge it, and resign, and let those act who can and will.

2-25-1848

White Water Valley Canal.

During the entire existence of the Board of Directors of the White Water Valley Canal, we have only been able to give the public account of its doings, from vague and uncertain rumor. When they have been acting in matters of more importance to the citizens of this valley, than any Presidential Election, they have not been disposed to furnish us with their action. We have therefore been compelled to catch up street rumors, &c. If in this way, we have fallen into errors, it was not our fault, as we have given it from the best sources at our command. As the interests of all in this valley are so intimately connected with their action, they must not be astonished at the anxiety to know what is doing —nor at our efforts to give all the information we have. If our information sometimes is erroneous, they are to blame, and not us. — Some one of them could always give the correct version. In what we said last week, we tho't our statements were correct, and regretted to have to make them. We are happy, however, to learn we were mistaken. Nor do we thing Mr. Shoup intends charging us with willfully misrepresenting. We do not so understand the following letter, for which we most heartily thank him, and hope he will hereafter keep us from falling into error, by furnishing us with the correct information:

C. F. CLARKSON, Esq. -- Dear Sir — I wish to correct the impression you are laboring under, in regard to the acts of the Board of Directors of the White Water Valley Canal Co. at their last meeting. You say the President, doubting the constitutionality of the law, hesitated until nothing was done, and the Board dispersed without any definite action. Such is not the fact. The Board promptly ordered an assessment of

THE WHITEWATER CANAL

10 per cent, equal to \$5 on each share of stock, to be paid on or before the first day of May next. This assessment will raise for repairs \$37,500. The stockholders will no doubt come up promptly to the requisition, and save their stock from sale, which the company have a right to do, under the late act of the Legislature, amending the charter of said Co. The above amount will enable the company to repair the canal from Brookville to Harrison. The board feeling assured that the above amount would be paid, accordingly ordered the engineer to advertise for the letting of the work on the 2d day of March, which will be done at the Canal Office at Connersville. I am no lawyer, and will not pretend to say whether the act is constitutional or not. But I will say, that I believe the assessment to be funded in justice and equity, and unless the stockholders do come forward and relieve the Co. their stock will and must be a total loss. Their own interest, as well as the interest of the State and the White Water country, demands it at their hands. I do not, for a moment, fear the result; the canal will be repaired. I regret that we have so many CROAKERS and FAULT FINDERS in our Valley, who are continually crying down the efforts of the Co. God knows the company have difficulties enough to encounter, without meeting the opposition of those who should be its friends. I would say to the stockholders, pay your five dollars on each share of stock you own, and do not suffer it to pass into the hands of others, who have done nothing to build the canal, and ultimately to reap the benefits of your investments. Yours truly,
GEO. G. SHOUP Feb. 18, 1848

3-10-1848

White Water Valley Canal.

The Board of Directors was in session last week. Every thing was portentous of a speedy repair of the canal. The contracts for repairs were about all let, to be finished by the first day of July next. The hands are already busy on some sections, making active preparations for work.

The dam at Harrison was let to Calvin & Co. The dam and lock at Case's was let to Wilcox & Reid. The dam at this place was let to Agasaelaus Rockafellar and Wm. Stewart, of Connersville. Mr. Stewart is the late publisher of the Connersville Telegraph. The embankment between the bridge at Brookville, and the mill was let to Elijah Moffitt.

It is understood that the Cincinnati Branch will be repaired by the time ours is ready for navigation.

The call on the stockholders for the amount necessary for the repairs, appears to meet with pretty general favor. We have heard of only one stockholder objecting — he having a large amount, and living at Laurel.

The board has determined not to rebuild

the embankment along the East river at this place, for the protection of those living in the part of our town lying east of the canal. They will have to build their own protection.

3-31-1848

The Canal.

Everything yet gives assurance that our Canal will be repaired this season. It is said they have determined to dispense with the Harrison dam the present year, and feed the canal all the way to Cincinnati from Case's dam —thus saving in the immediate repairs some 15,000 dollars. This will be rather adverse to Lawrenceburgh interests, but that is their business. Hands are congregating along the line — thtimber and stone are moving, and every thing looks like prosperity. It makes us glad.

4-21-1848

Geo. Holland of this place, and James Woods of Henry Co., have been appointed Directors of the White Water Valley Canal in the places of S.W. Parker, (and some other one) who have resigned.

The board has concluded not to make the bridge at this place a toll bridge, if our county commissioners will agree to keep it in repair. —We hope our commissioners will not agree to any such condition.

5-24-1848

White Water Canal.

The Board of Directors of the White Water Valley Canal, met in this place on Monday last for the transaction of the general business of the Company, and for the purpose of disposing of the stock of such Stockholders as had not paid in their assessment.

Among the orders of the Board was one authorizing the engineer of the Company direct the contractor of the Laurel aqueduct to proceed with the work on the same so soon as the citizens of Cambridge City and vicinity raise \$1000, the citizens of this town and vicinity \$1000, and the citizens of Laurel and vicinity \$500, to be appropriated to repairing said aqueduct; and that one-half the tolls at the office at Cambridge and Laurel be pledged until the construction of said aqueduct be fully paid for and also that the tolls be pledged after the first day of January next, until the advances of said citizens be paid.

Tuesday being the day set for selling delinquent stock, for failure to pay the assessment, the Board proceeded to the sale when upwards of 70 shares were sold. — We believe it was all purchased by Mr. A. B. Conwell of this place. The Stockholders have leave to redeem the stock sold, by paying the amount of assessment with ten per cent damage on the same, providing it is paid within sixty days from day of sale.

About \$33,000 of the assessed tax was paid in on Monday and Tuesday; and the first estimate upon the contracts recently

let by the Company, is to be paid some time this week.

Geo. Holland, Esq., who was elected a member of the Board at its last meeting, declined to serve, and James Raridon, of Cambridge was chosen to fill the vacancy.

We congratulate the members of the Board and the friends of the Canal throughout this county, at the cheering prospect there is at this time that the Canal will be speedily repaired and navigation opened. The majority of the Stockholders have come up nobly and paid their assessment and we confidentially expect that much of the stock sold will be redeemed before the expiration of the sixty days.

Nothing is now wanting but more hands to do the work. The contractors will push the work with renewed vigor, under the assurance that the estimates will be regularly paid.

New life has been infused into the Board by the promptness with which the assessed funds has been paid, and at the cheering prospect. There is no longer a doubt, or a shadow of doubt about the Canal being repaired —and that permanently too. We calculate with great confidence on it being opened in time for the fall business. Go ahead, friends, and let us have the Canal, and no longer will business droop. It has for several months past. Our streets will no longer wear a dull appearance, but be crowded with wagons of wheat and the surplus products of the rich farms of this beautiful Valley —Instead of pits and pools of stagnant water and miasmatic wastes, enlivened by thousands of frogs, will be heard the glad notes of business and prosperity. — *Connersville Telegraph*

1849

7-20-1849

W. W. Canal — Heavy Rain.

From the Cincinnati Times, July 12

The rain which fell last night must have done much damage to the White Water Canal. We learn that the embankment gave way in two places this side of Mill Creek, and the water ran over the bank just above and below Mill street, furnishing the residents in that vicinity with more water than they stood in need of. Men were busily engaged this morning, under the direction of Henry Noble, councilman, in draining the little lakes and conducting the water back to its right channel. The rain commenced falling about 12 o'clock, and continued until 3 in the morning, and we have seldom, if ever, seen so much rain fall in so short a time.

Mr. Yeatman, the President of the White Water Canal Company, called on us this morning, to inform us that the canal fortunately broke in three places, and that during the hardest of the rain, the superintendent near the city got up and opened the waste ways. These openings saved the canal from

THE WHITEWATER CANAL

a vast amount of damage; as it is, the repairs can be made in about one week, and sooner, if it were not that dirt is scarce, and has to be carted some distance.

9-7-1849

Navigation Open

FARE REDUCED & SPEED INCREASED.

FROM and after Thursday, 6th Sept., 1849, until further notice, one of G. W. PAVER'S White Water Canal Packets, fitted up for comfort, and with taste, will leave the Basin between Western Row and John sts., Cincinnati, O., for Cambridge City, Ind., at 4 o'clock P.M. precisely, passing through Industry, Cleave Town, Harrison, New Trenton, Rochester, Brookville, Metamora, Laurel, Connersville and Milton. The following boats comprise the line:

JOHN Q. ADAMS, Capt. James Russell;
CLARA " Jesse Russell;
TEMPEST " G. W. Paver.

These Boats are exclusively for passengers. — Every attention will be paid to the comfort of all passengers who travel on these boats.

Sept. 4 — 37-3w G.W. PAVER

10-5-1849

Whitewater Canal.

We regret to be compelled to use our standing article, and "announce a breach in Whitewater Canal, and the assurance that repairs will be made and the work in order for boats on Thursday." The storm of Sunday made this breach near Mr. Shorts, about 1¹/₂ miles below the city.

This breach is the more to be regretted, as we have to announce the repair of the Laurel (IA.) aqueduct, and that if hot for the above breach boats would go direct through to the place.

After Wednesday it is believed there will be a through navigation.
Cin. Gazette, Oct, 2d.

10-12-1849

A New Canal Feature.

We understood, on Saturday last, from Captain Paver, that the first packet boat had arrived here on that day by way of the White Water Canal, that ever floated from Hagerstown, which place is situated seven miles beyond Cambridge City, since the canal was made navigable. —There is now an uninterrupted navigation as far as said place, and the canal is in good order. The travel on Capt. Paver's Packets is increasing rapidly, and the merchants and others of that part of Indiana are coming here for supplies. Cin. Com. Oct. 8

11-9-1849

Navigation to Terre Haute.

On Thursday morning last, two canal boats entered Terre Haute for the first time, whereupon the citizens of that beautiful town cut up sundry capers by way of a celebration. Speeches were made — can-

nons fired — and considerable good liquor wasted on the occasion, We hope they had 'a good time of it.' — Tip. Jour.

11-16-1849

The Canal.

This public improvement, which many of us begin to look upon more as a curse than a blessing, is still obstructed, but we have promise of its being filled again the beginning of the week. Did we not dread a merciless onslaught, like unto the Mt. Carmel affair, or some thing worse if possible, we would attempt to show that there is some reprehensible conduct in this matter. — But as we have learned from experience that it will not do to speak of any man's faults, we forbear. When we find preachers and people will write, and certify to a lie, in order to cover up wickedness and corruption, it is easy to learn editors to leave the public to seek its own redress from wrongs. We like peace and good will as well as any one, and a few such cases as that at Mt. Carmel, where the citizens certified to that which they knew was false, in order to shield a delinquent, we reasonably expect the same will be done in other cases. Consequently if the public wish their wrongs redressed, let them do it themselves. And the very man that now calls upon us to expose this matter; once united with a corrupt clique against us because we exposed another man's rascalities. Mr. Carmel does not contain the only lying committee that we have found in our editorial career. We could stir one up almost any where, if we should disturb the dark and malignant bile of old offenders. It is true its members might not stand as high in the church, but we have found that malice, hatred, malignity, self-interest and popular fury, will raise a lying committee in any little town in the land. Some times we are untho'tedly and innocently brought under their displeasure. We therefore look at their productions as lies merely got up to sustain their side of the question.

We therefore decline the urgent request of our friend and correspondent. We cannot do up the dirty work of every one, and then be left alone to beat off the hounds.

11-30-1849

Our Canal.

In all departments of trade we find occasionally men of enlarged and liberal views — men who can comprehend the principles upon which the laws of the trade and commerce operate. These men are blessings and ornaments to the world. But such men are seldom allowed to control or direct for any length of time, any of our STOCK companies. Stocks are taken and owned, generally, for the amount of per cent at the dividends, without regard to the public good. And their directors are so sordid, selfish and grasping, that they seldom see or comprehend that which will lend to their own

good. —The consequence is that SHAVERS control our banks, MUSKRATS our Canals, and SNAKE-HEADS our Railroads, until there is a popular fury raised against corporations. We have never joined in this crusade, nor held sympathy with those who do. We confess, however, our faith in their purity and utility is occasionally somewhat shaken.

Our attention has been particularly called to this subject by the unwise, ruinous and narrow-minded policy that the mousing muskrats are pursuing, who control and direct our canal. — It is true, from their sad reverses, they have some claim on the public sympathy and the public purse. But this is no reason that they should kill at once the golden egged goose — drive all business from the canal, and the towns along the dirty ditch, and beggar its merchants. This they are most efficiently doing as present. The tolls are now so high, that two thirds of the business of our valley is done in wagons. The boatmen, who begin to look lean and lank, are becoming ravenous, and it is becoming impossible for any one to keep up a boat, unless they pay their hands in grog, filched from the whiskey freight; and steal chickens and turkeys along the line, to board their hands, and corn from the fields for their horses.

Let any one go to Main or Pearl street, Cincinnati, and he will see how the goods are now brought to this valley. It is done in the old fashioned way, by wagons, for the reason that the exorbitant tolls cannot be paid. But the way business is now done is starving boatmen, ruining the profits of the canal, and dividing the business between boatmen and teamsters.

The present course of policy is such that it is driving the business from the towns along the canal. The freights are so high that our merchants cannot compete with neighboring rival public works -- & consequently farmers to other points to sell their produce, and when there to purchase their goods. The merchants and mechanics along this valley are vitally interested in this question, and it is time they were speaking in tones to be understood upon the subject.

Without any personal allusion, we do think the present directors are manifesting as little liberality and as little general sense as any body of men that we know entrusted with so important a charge. They are truly a set of grasping, narrow-sighted muskrats. These are our sentiments.

What we say is for the good of the canal —for our good, and the public good. In the matter, we have no foes to punish or friends to reward.

1850

3-8-1850

Another Flood.

It commenced raining about eight o'clock on Tuesday night, last, on a coat of

THE WHITEWATER CANAL

snow then covering the ground, which continued for about 16 hours. The consequence was, the two branches of White Water were too full to contain themselves, and have committed various trespasses upon the canal, as well as the lands and tenements of peaceable people. In various places has the water, like Gen. Jackson, removed the deposits without law or a due regard to the wishes or interests of the people. The rushing water, in its great haste to reach its home in the ocean, has cut up various antic tricks, enough to make a canal stock-holder weep, whilst those who have been borne down by the lever of oppression, which the company have used, so unmercifully for the last year, will be disposed to laugh.

The water rose to within 22 inches of the flood of Jan. 1st 1847. We have heard of a few slight breaks in the canal, which can be repaired in a few weeks. We have not however heard any distance along the line, as the floods have stopped the mails. Speer & Co., who have heretofore suffered so severely at each successive flood have escaped with slight damages this time. One of the abutments of their dam was undermined, and one half torn away, but as the dam is safe it will not interfere with the business.

Thursday evening. — We learn that about 100 feet of the canal dam, 4 miles below town, is gone. The part gone commences about 100 feet from the eastern abutment. It is of the new part, which was put 18 months ago. This will take some months to repair as it cannot be worked at until the season of low water. The canal bank is more or less injured all along between here and Rochester. From this to Laurel the damage can be repaired with \$500 to \$8090. Above Laurel, there is a break at Garrison Creek. We expect to hear of more damage below than above, as the flood was principally in the Southern end of the River.

5-10-1850

The Canal. The Cambridge Reveille has stated that the Canal probably would be ready for navigation by the 10th day of May. This day has arrived, and the dam below this place has not yet begun to rise above the water. In fact, the water is yet 17 feet deep where the dam is to be built. We would think it best not to misrepresent matters in this way. Commercial men, depending on the canal, wish to know when it will be repaired. We do not charge the editor at Cambridge with misrepresenting — he doubtless received his information from some one of the directors. It has been republished in all the papers in this region, and it has misled people.

5-31-1850

White Water Canal. — We are informed that the dam below this place is about com-

pleted, and that the water has already been let into the Canal. There are strong anticipations of boats reaching Cincinnati in at least ten days, without unforeseen delays, and we think businessmen can rely on this period as the commencement of Navigation.

6-7-1850

The water has been let into the canal from Cambridge City to Harrison; but the dam at Harrison is in such a leaky condition that we have no water in this end of the canal. Twenty-five hundred yards of gravel would repair the dam, yet the company assert their inability to pay it and are talking of begging the money of this place soon. Our citizens raised them a large sum of money once and have not had two weeks of water since, unless it was when the White Water river was so high that the water could not be kept out. — Law'h Reg. June 1st

Independent Press Lawrenceburg

11-15-1850

Imports

By Lewis & Eichelberger 2400 bushels wheat by canal, 2000 bushels do from Madison, 3600 bushels do by wagon, 1150 flour barrels by do.

Exports

By Lewis & Eichelberger 1400 barrels flour to New Orleans by Campbell's flat boat, 500 bushels coat to Harrison, by canal boat York.

11-22-1850

SHIPMENTS.

Lewis & Eichelberger 2679 bbls four to New Orleans, per Campbell's two flat boats

The White Water Canal is in good order, and boats continue to arrive and depart, but we may, from present indications, soon look for a freeze up.

11-28-1851

A wagon was upset in the White Water canal in Storrs Township yesterday. Two men were in it at the time and one of them was drowned. The men were intoxicated and the horse they were driving blind.

CANAL MENTIONED IN 1844 LETTER FROM CONNERSVILLE

H.C. Moore wrote a letter to Daniel Agnew Esq., Attorney at Law, Beaver, Beaver County, Pennsylvania, on January 22nd, 1844 that mentions the Whitewater Canal and the Cincinnati & Whitewater Canal as well as discussing mutual friends, family and politics. A portion of the letter is quoted here.

"The banks of the Ohio are beautiful and our Hoosier Country will show to a good advantage. If we should happen to have such an influx of pigeons as we had last April & May you might luxuriate with a good shot gun. A man three miles above town caught 100 dozen in a net and took them to Cincinnati, where I believe he sold them at 50 cents to \$1.00 per dozen. We finished 14 miles more of our canal last fall and now have it navigable from Lawrenceburgh and also from Cincinnati to within 12 miles of this place."

The letter also contains a discussion of the increases in farm crop prices including hogs. Moore is bullish on the prospects of a strong regional economy. He discusses politics, saying, "The Whigs are sanguine of giving the vote of the state to Mr. Clay, and I have no doubt they will — we will have another exciting campaign, perhaps equal to that of 1840."



THE WHITEWATER CANAL

Cambridge City Tribune

3-29-1934

BABY DROWNS IN CANAL

Parents, Brother Reach Safety After Auto Submerges

Milton, Ind., March 28 — Vada Zell Cooney, 11 month old daughter of Mr. And Mrs. Carl Cooney of Connersville, former Milton residents, was drowned in the White-water Canal late Sunday night in an automobile accident in which her parents and brother Christian escaped injury. The Cooney car overturned into the canal near the Dentilinger bridge north of Connersville when the family was returning home from church services at Cambridge City. It was thought Mr. Cooney became blinded by the lights of a passing car and pulled out on the bank too far.

Mrs. Cooney and the boy swam to the canal (bank) and were helped out by a passing motorist. Mr. Cooney also managed to escape from the submerged car, but the body of the baby could not be found for almost an hour, when it was located about a mile downstream from the place of accident.

Besides the parents and brother, the grandparents Mr. And Mrs. Griff Cooney of this place, and Mr. And Mrs. James B. Murley of Columbus survive.

Funeral services were held Wednesday afternoon at the Nazarene church in Cambridge City. Burial in Riverside cemetery.

9-18-1936

Omer McWilliams, born 29 Jan 1863, came to Cambridge City when 3 years old and is now oldest colored person in the community. He picked poultry for 53 winters, was a cook in Chicago for 23 summers and one summer was a dining room cook on the Illinois Central Railroad.

During canal days he kept the "toe" path and canal clean from Cambridge City to Beeson's Sta-



BROOKVILLE BASIN

Center: Ice cutting at the Canal Basin in Brookville, Indiana, Feb. 9, 1908, taken by Ben Winans. Note road up the hill from the basin. Ice was cut in the basin during canal times.

Bottom: Looking south from the hill towards Basin area, Second St. and road to Harrison. March 24, 1898 and were taken by Jacob Harry Swayne. This is similar to the major floods during canal times in Brookville,

tion. At one time 200 colored families lived in Cambridge City, and colored camp meetings were held south of Cambridge City in Ferris Woods.

9-18-1936

A second story in the same paper says that the Cambridge City to Cincinnati packets left Cincinnati every day of the week at 2 p.m.

and arrived in Cambridge City during the afternoon of the next day. The same boat left Cambridge City the next morning at 7 a.m. and took about 24 hours to return to Cincinnati. Cost was \$3.00 each day, including meals. McClure and Dickey were the agents at Cincinnati with their offices at the White-water Canal Basin. A. Reiser at the Mansion House here looked after

THE WHITEWATER CANAL

the Cambridge City end of the performance not on the pro- Johnson, Will Gaar, Fred Farquhar, business. gramme. They went on the railroad Harry Yeo, Fred Haner and Sol.

Mail for the East was taken as far as Cambridge, and from Klein of Richmond. George Sinnet, from Muncie to Cambridge City by there down to Milton in Wagons. of Sevastopol, was the only one stagecoach, then to Cincinnati by One large wagon contained some from here who was disabled. One canal boat, and then on East. In ninety persons, and brought up the of his legs was wrenched so badly addition to other freight, there was rear of the procession. The road is that he could not walk, and he considerable salt pork carried by along the towpath of the old was taken back to town in a the canal boats from Wayne Whitewater canal. In order to wagon, together with a Cambridge County to Cincinnati according to catch up with the other wagons man whose ankle was dislocated. the *Muncie Journal*. the driver of the big team whipped Nearly all the rest continued on to up, and had his horses going at a Milton, though tow or three had lively run when the wagon upset lost their shoes in the mud, and and spilled the boys into the bed of others their caps and torches. Mr. the ragin "canal," raging mostly Roulke was bruised; Mr. Linden- with tad-poles as the mud was muth's nose considerable tar- deeper than the water. Among nished, Cal Johnson and Fred Far- those who were completely im- quhar each had a finger mashed. mersed in the doubtful fluid were Fred Farquar, Will Gaar and Henry boys who went over to Milton last Henry U. Johnson, W. Dudley Yeo walked back to Cambridge af- Tuesday evening took part in a Foulke, A. C. Lindenmuth, Cal. ter the accident.

Richmond Paper

Late 1800s

BAPTIZED

Richmond Garfield Guards Take a Dip in the Canal.

A number of the Richmond boys who went over to Milton last Tuesday evening took part in a

NOTES

The White Water Canal will make this valley the richest and most independent part of the world. We are yet young, but with our fertile soil — rich and inexhaustible — there is nothing to impede our progress to wealth the most unbounded. When we recollect what has been done in this valley within the last twenty-five years, who can estimate the wealth, splendor and magnificence, which shall beautify and adorn this valley in fifty years. Mansions and palaces will displace the neat cottage farm house, as they have driven out the log cabin, and our posterity will enjoy in luxury our labors and the toil of our fathers. But without some great public improvement, we could never have been any thing but hewers of wood and market peddlers.

4-25-1845 *Brookville American*